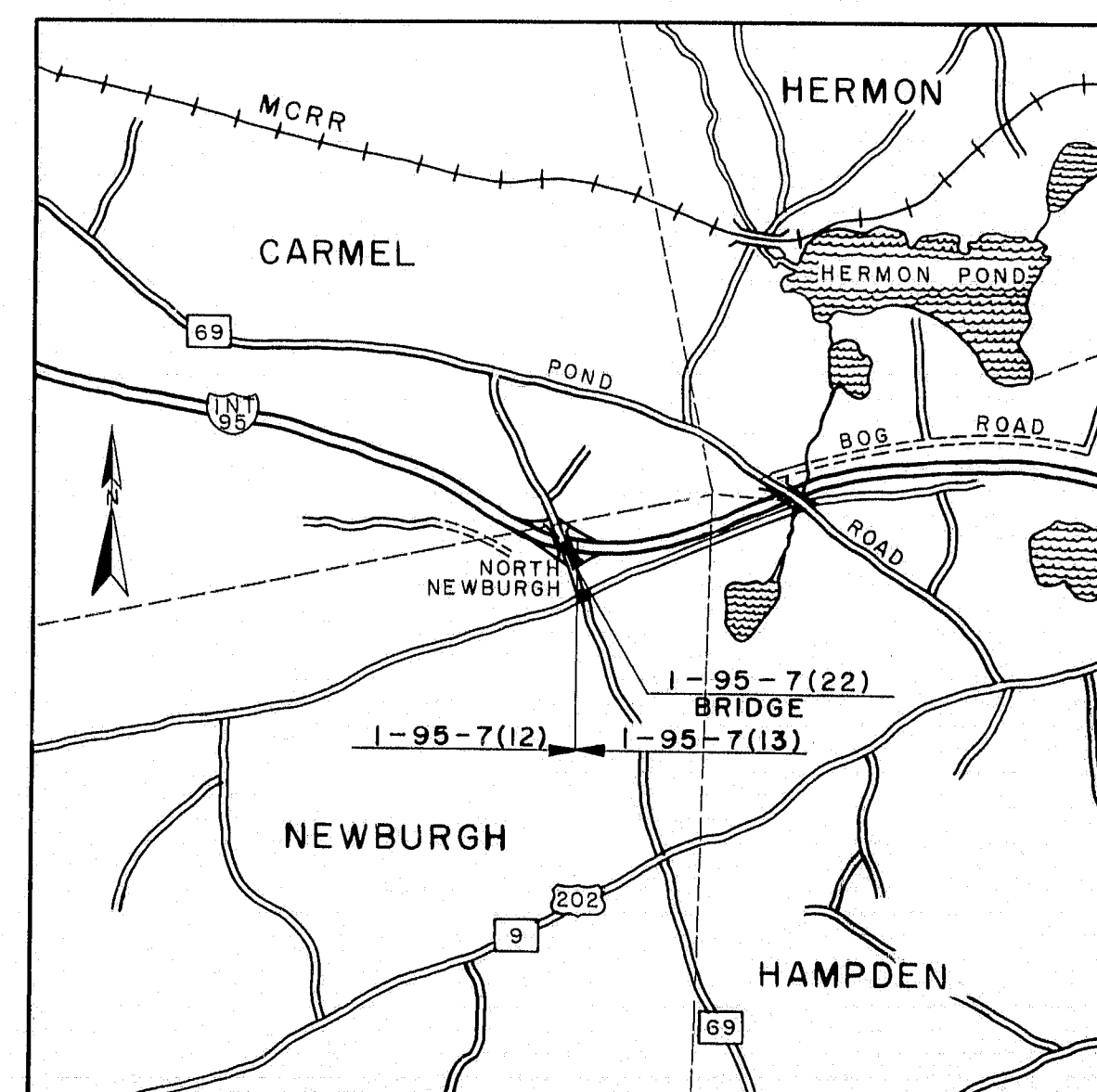


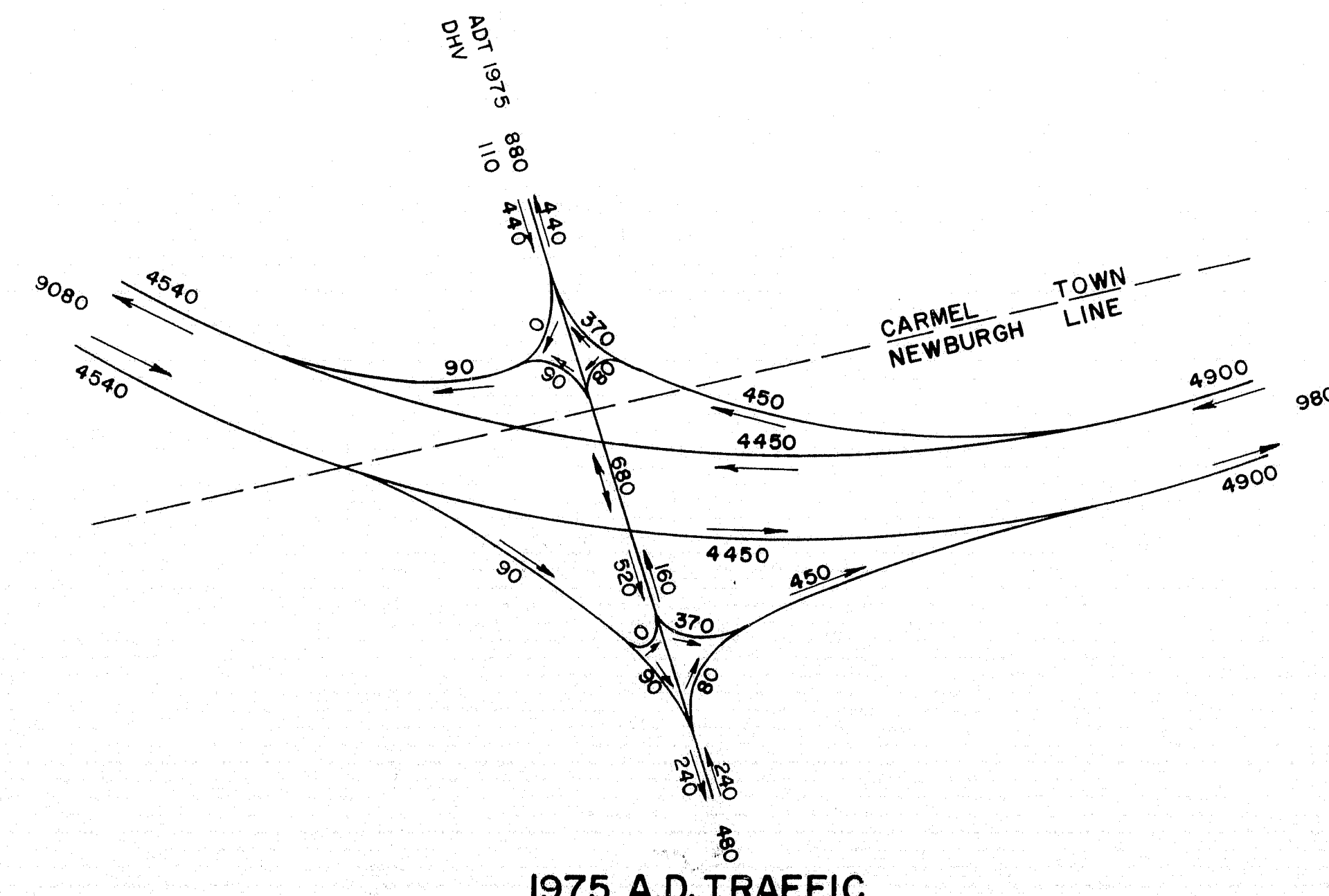
STATE OF MAINE
STATE HIGHWAY COMMISSION



ROUTE 69 BRIDGE
OVER
INTERSTATE HIGHWAY 95
IN THE TOWN OF
NEWBURGH
PENOBSCOT COUNTY
FEDERAL AID PROJECT NO. 1-95-7(22)167



LOCATION MAP
APPROX. SCALE - 1" = 1 MILE



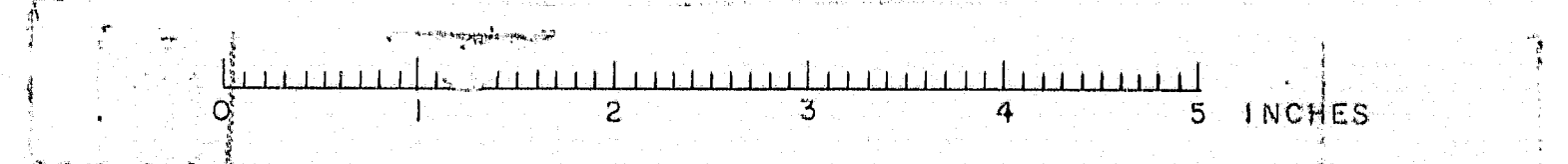
1975 A.D. TRAFFIC
T = 11 %
D = 60 %
V = 50 M.P.H. (ROUTE 69)

APPROVED
MAINE STATE HIGHWAY COMMISSION

David H. Brown
CHAIRMAN
Robert L. Brown
- *John L. Milham*
CHIEF ENGINEER

MARCH 28 1962
DATE

DEPARTMENT OF COMMERCE
BUREAU OF PUBLIC ROADS
REGION 1
APPROVED
DIVISION ENGINEER DATE



SPECIFICATIONS

DESIGN - A.A.S.H.O. STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES 1961.

CONTRACT - STATE OF MAINE, STATE HIGHWAY COMMISSION, STANDARD SPECIFICATIONS HIGHWAYS AND BRIDGES, REVISION OF JANUARY 1956, AND SUPPLEMENTAL SPECIFICATIONS.

LIVE LOADING

H20-44

ALLOWABLE LOADING

STRUCTURAL STEEL A.S.T.M. DESIGNATION A36 - $f_y = 20,000$ p.s.i.
STRUCTURAL STEEL A.S.T.M. DESIGNATION A7 - $f_y = 18,000$ p.s.i.
REINFORCING STEEL, INTERMEDIATE GRADE - $f_y = 20,000$ p.s.i.
CONCRETE ($n = 10$) $f_c = 1200$ p.s.i.

CONCRETE CLASSIFICATION

ALL CONCRETE -----, CLASS "A"

ELEVATIONS

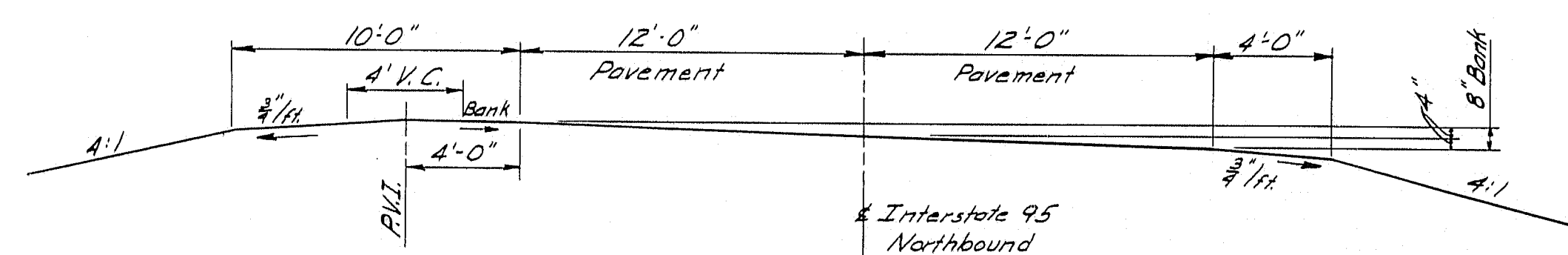
ELEVATIONS ARE BASED ON B.M. #136, R.R. SPIKE IN 14" ELM, STA. 3342+16 N.B. OFF. 228' R, EL. 170.40.

ESTIMATE OF QUANTITIES

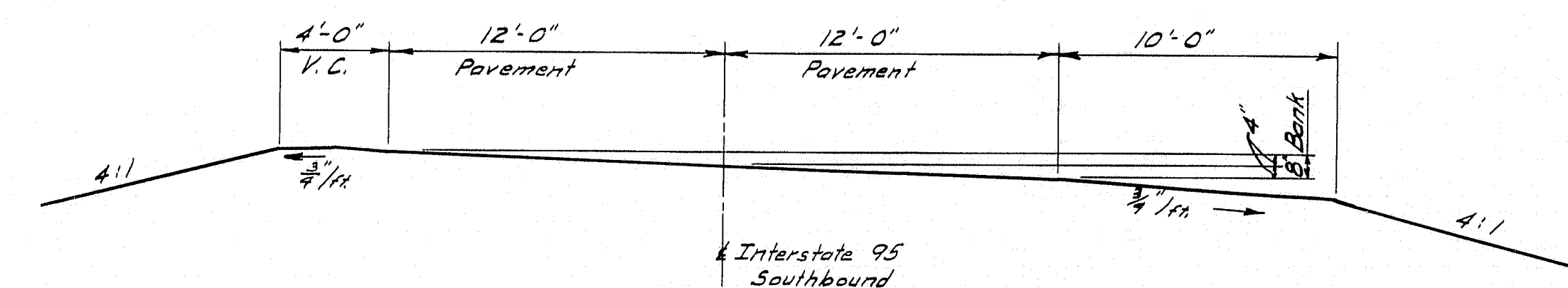
ITEM	UNIT	QUANTITY
STRUCTURAL EARTH EXCAVATION - PIERS	C.Y.	165
COMMON BORROW	C.Y.	5400
GRANULAR BORROW	C.Y.	330
GRAVEL BASE COURSE- IN PLACE MEASUREMENT	C.Y.	50
BITUMINOUS CONCRETE SURFACE COURSE - TYPE "A"	TONS	150
PORTLAND CEMENT CONCRETE - ABUTMENTS & RETAINING WALLS	C.Y.	180
PORTLAND CEMENT CONCRETE - PIERS	C.Y.	275
PORTLAND CEMENT CONCRETE - ROADWAY & SIDEWALK SLABS ON STL. BRIDGES	C.Y.	455
PORTLAND CEMENT	BBLs.	1370
STRUCTURAL STEEL - FABRICATED & DELIVERED	LBS.	386500
STRUCTURAL STEEL - ERECTION	LBS.	386500
STRUCTURAL STEEL - FIELD PAINTING	LBS.	386500
REINFORCING STEEL - DELIVERED	LBS.	149700
REINFORCING STEEL - PLACING	LBS.	149700
STEEL H-BEAM PILE 42 LBS. PER FT.	L.F.	1290
ALUMINUM RAIL	L.F.	900
MEMBRANE WATERPROOFING	S.Y.	1340
EPOXY RESIN SURFACE SEALANT	S.Y.	90
SLOPE PAVING	S.Y.	500
GRANITE BRIDGE CURB	L.F.	915

INDEX OF SHEETS

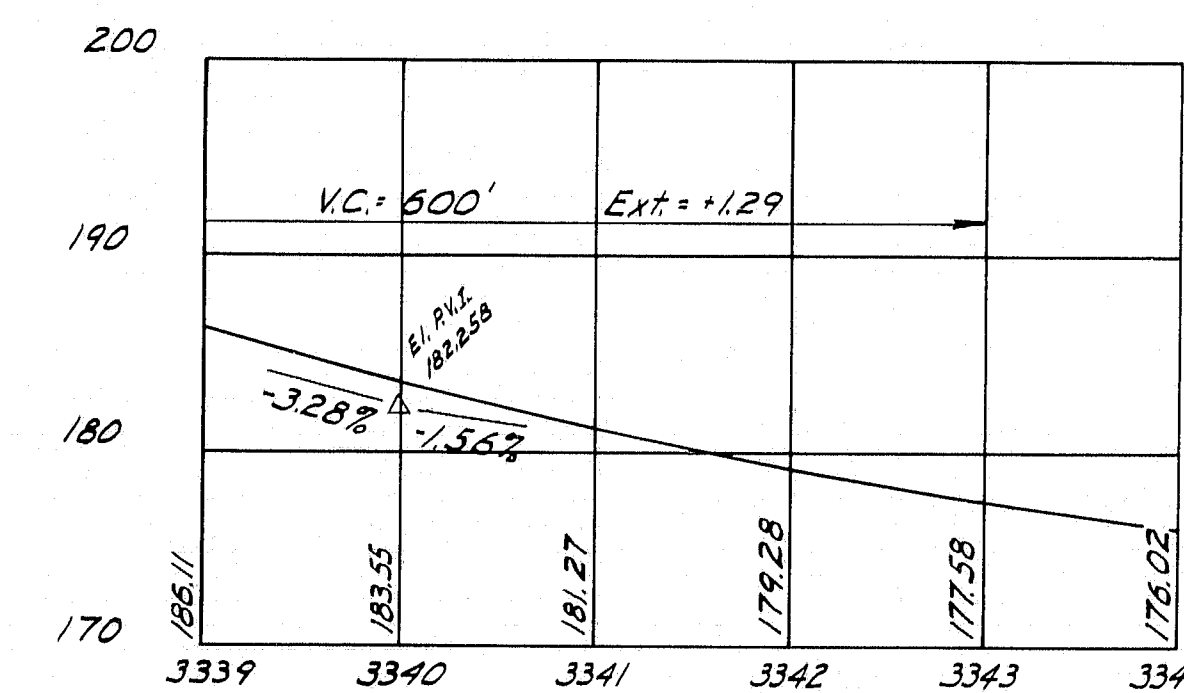
SHEET NO.	TITLE
1	TITLE SHEET
2	SPECIFICATIONS, QUANTITIES, INDEX, PROFILES
3	FOUNDATION SURVEY
4	BORING DETAILS
5	GENERAL PLAN & ELEVATION
6	ROADWAY WORK & SLOPE PAVING
7	ABUTMENTS
8	PIERS
9	STRUCTURAL STEEL - FRAMING PLAN
10	STRUCTURAL STEEL - DETAILS 1
11	STRUCTURAL STEEL - DETAILS 2
12	BLOCKING DETAILS
13	SUPERSTRUCTURE - SPAN 1 & 2
14	SUPERSTRUCTURE - SPAN 2 & 3
15	SUPERSTRUCTURE - SPAN 4 & 5
16	SUPERSTRUCTURE - SPAN 5 & 6
17	ALUMINUM RAIL
18	REINFORCING STEEL SCHEDULE



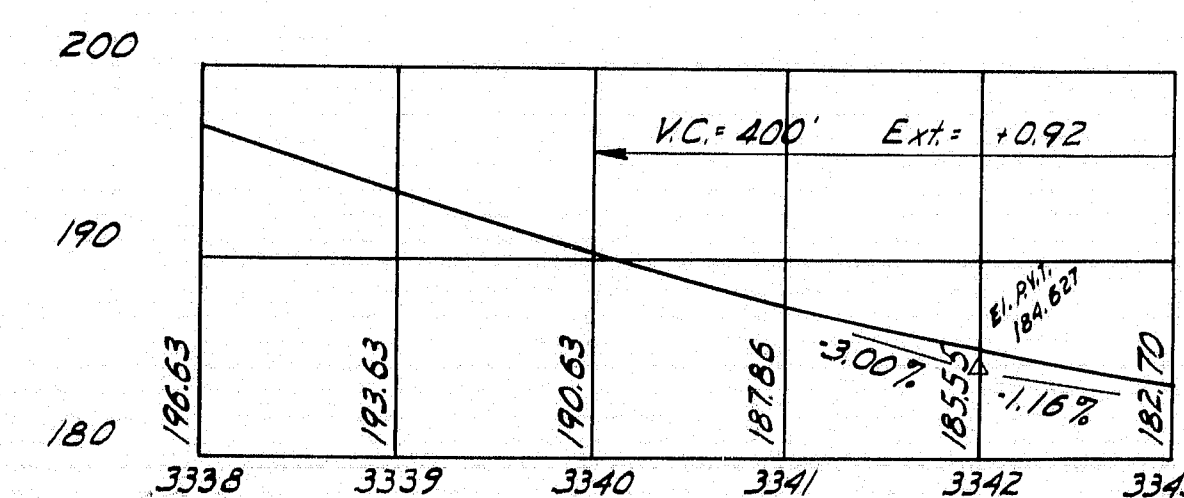
CROSS SECTION INTERSTATE 95
NORTHBOUND LOOKING WEST



CROSS SECTION INTERSTATE 95
SOUTHBOUND LOOKING WEST



NORTHBOUND



SOUTHBOUND

PROFILES - I-95

Scales: Vert - 1" = 10'
Horiz - 1" = 100'

CURVE DATA I-95 N.B.

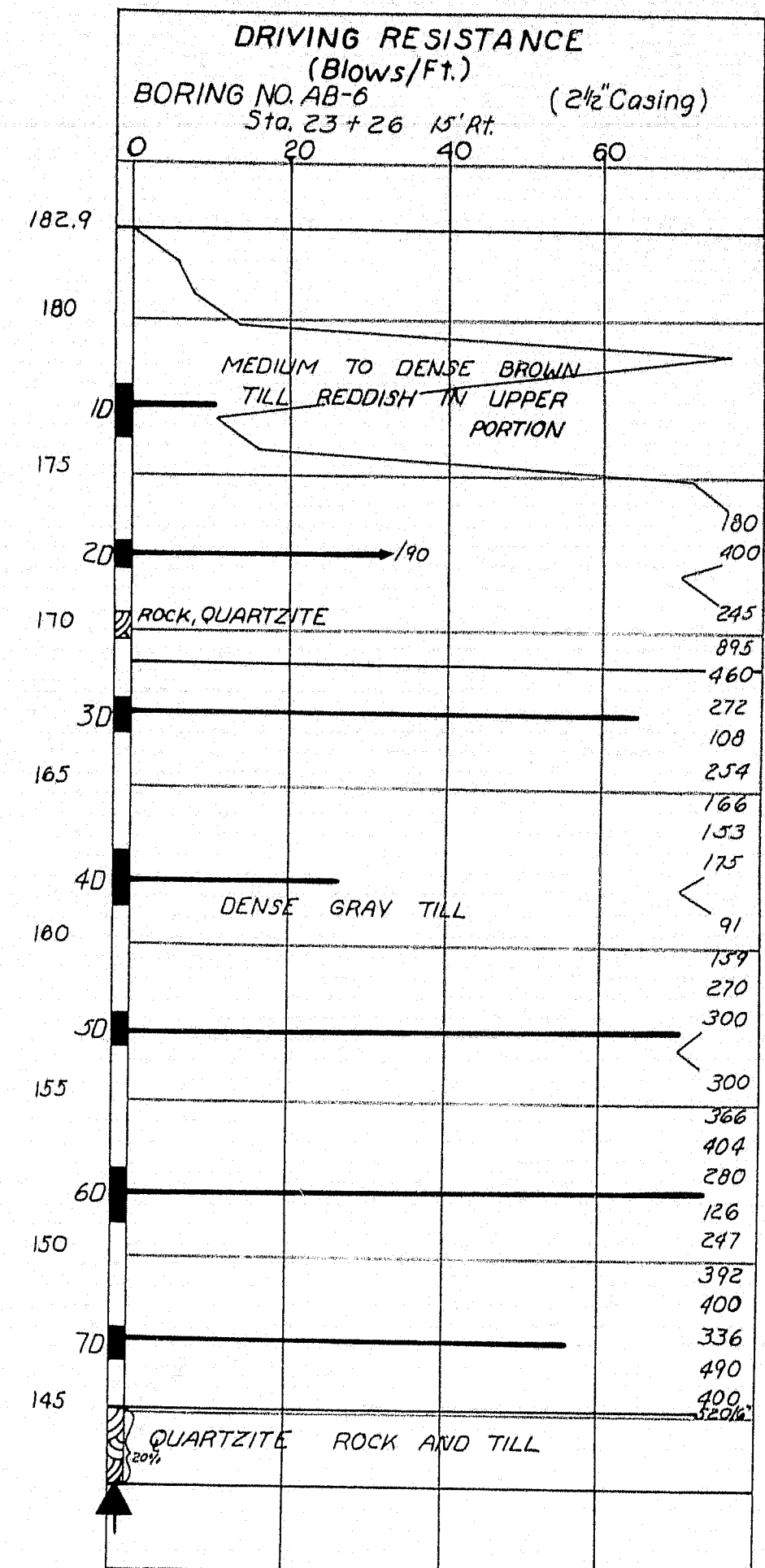
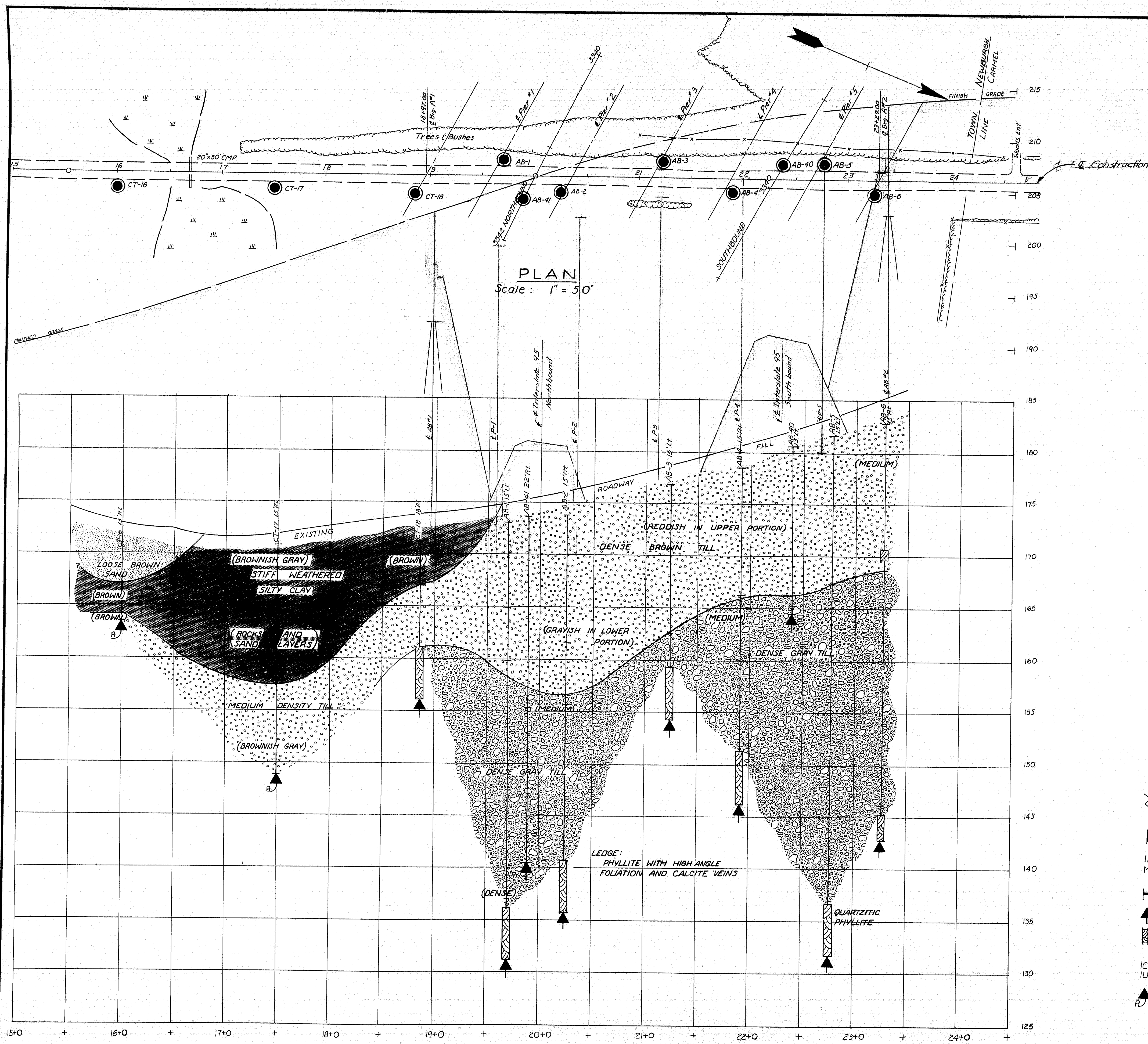
Sta. P.I. = 3337+82.90
Δ = 52°-54'-08" Lt.
D = 1°-12'
T = 2375.47'
L = 4408.52'
R = 4774.65'
E = 558.28'

CURVE DATA I-95 S.B.

Sta. P.I. = 3340+55.30
Δ = 52°-51'-48" Lt.
D = 1°-15'
T = 2278.51'
L = 4229.06'
R = 4583.66'
E = 535.08'

DESIGN - CDH DET. JHW
TRACE - JHW
CHECK - JHW
BRIDGE NO. 69
SURVEY -
PLOT -
STATE HIGHWAY COMMISSION
BRIDGE DIVISION
ROUTE 69 BRIDGE
OVER
INTERSTATE HIGHWAY 95
IN THE TOWN OF
NEWBURGH
PENOBSCOT COUNTY
SPECIFICATIONS, QUANTITIES, INDEX, PROFILES
SHEET 2 OF 18 AUGUSTA, MAINE MARCH, 1962

85-160



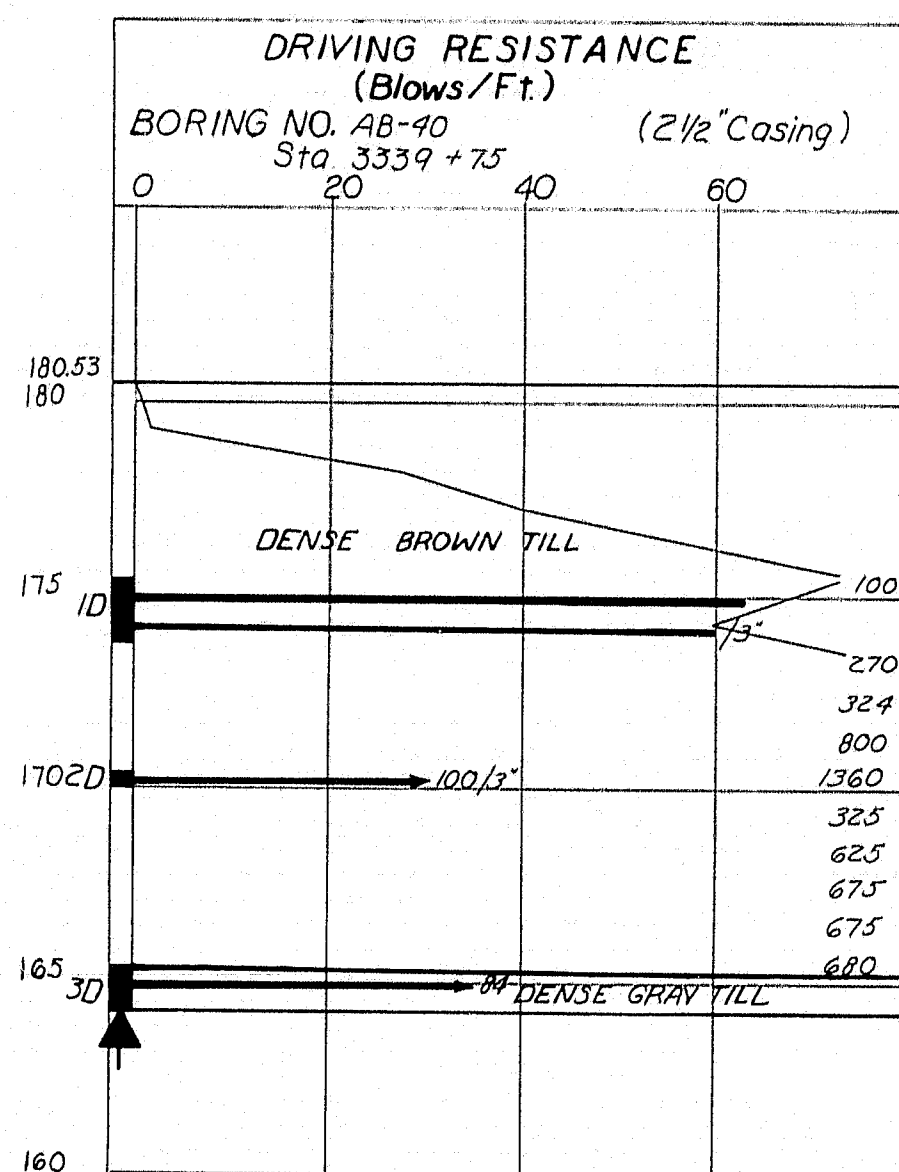
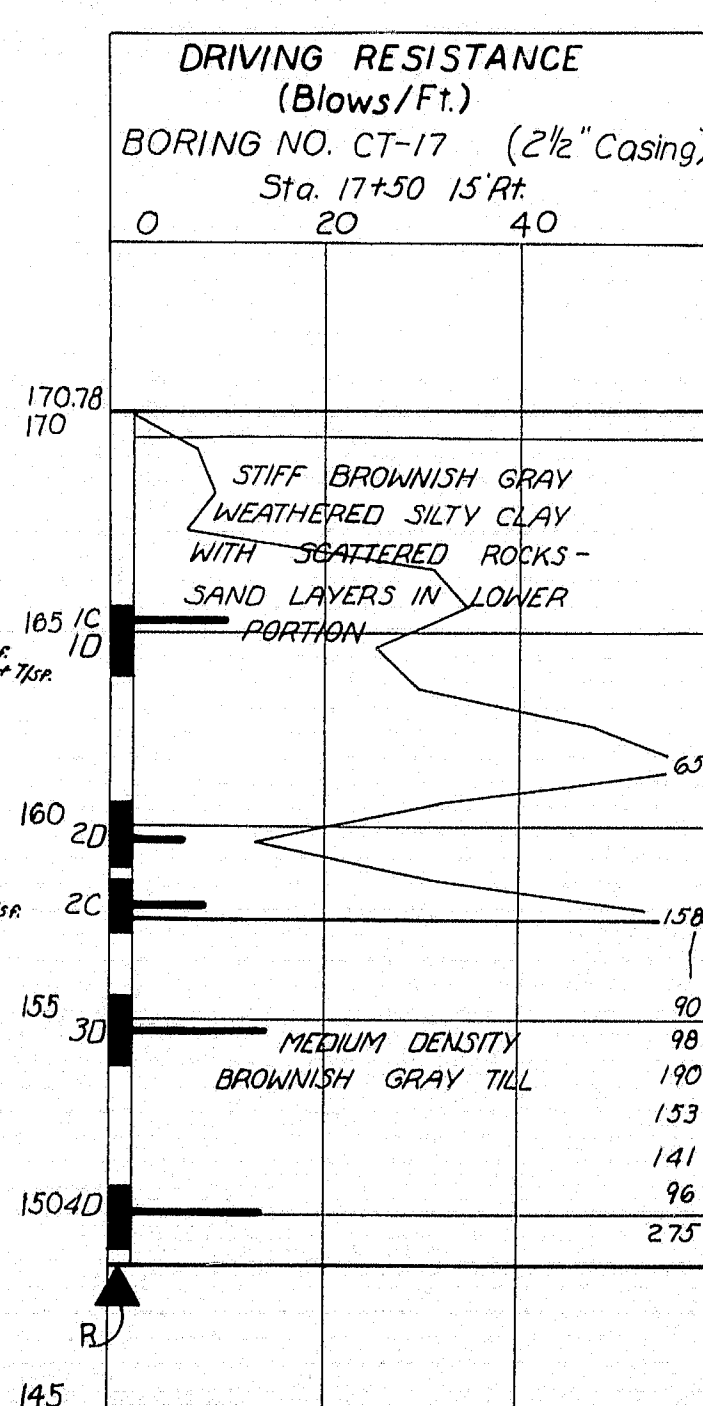
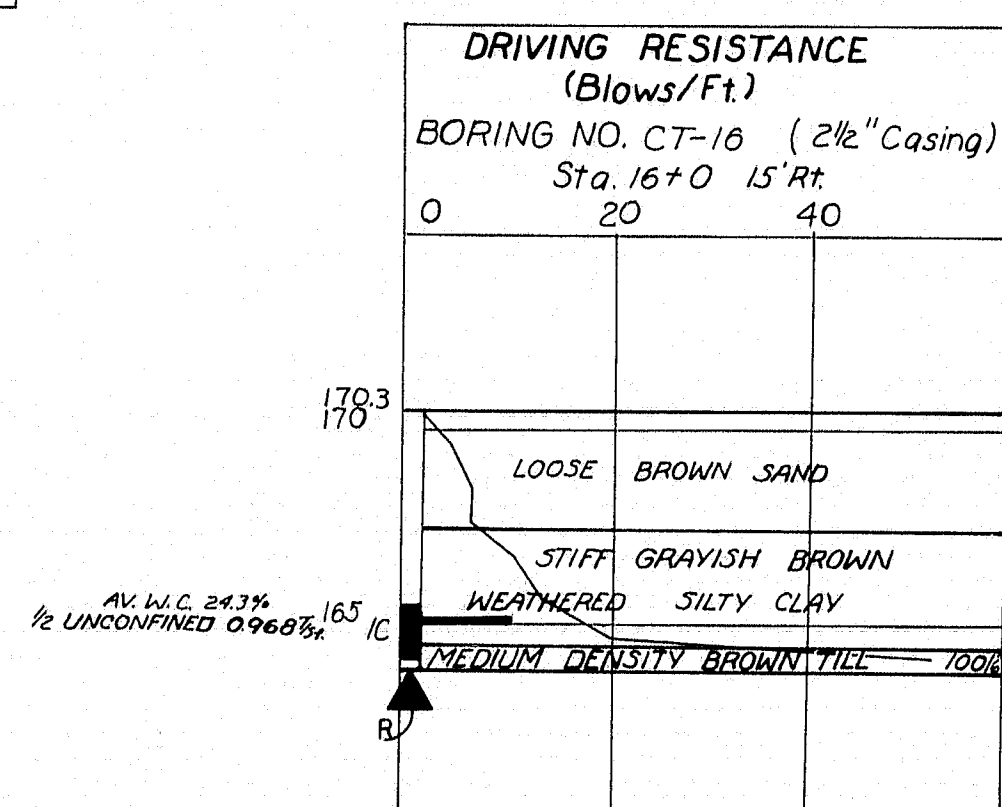
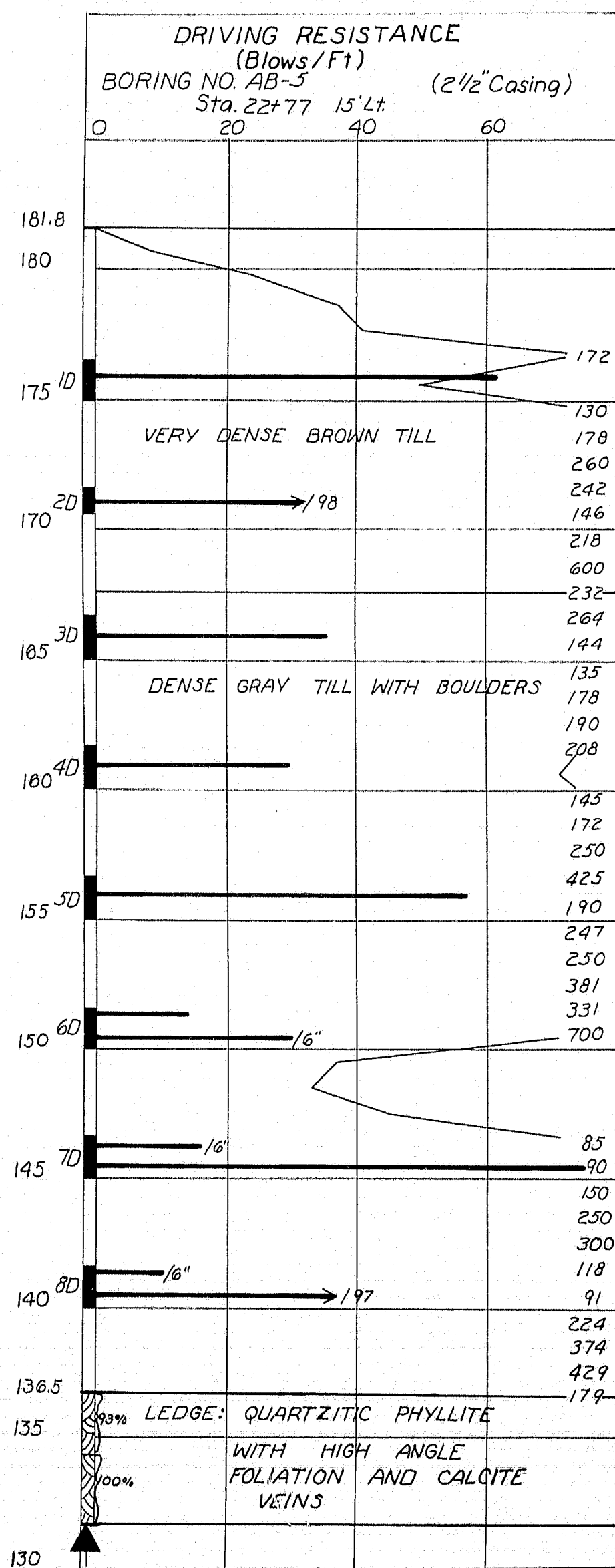
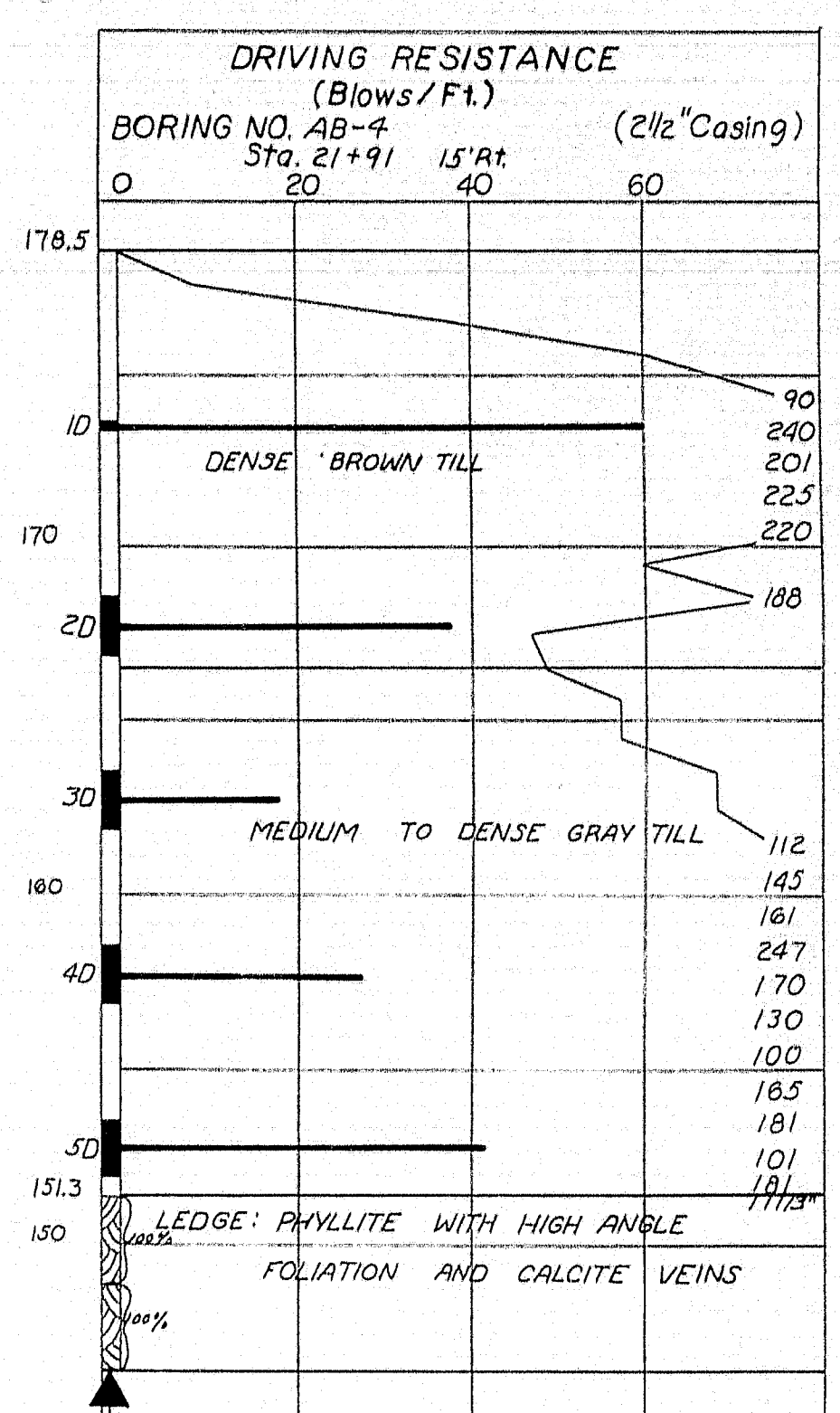
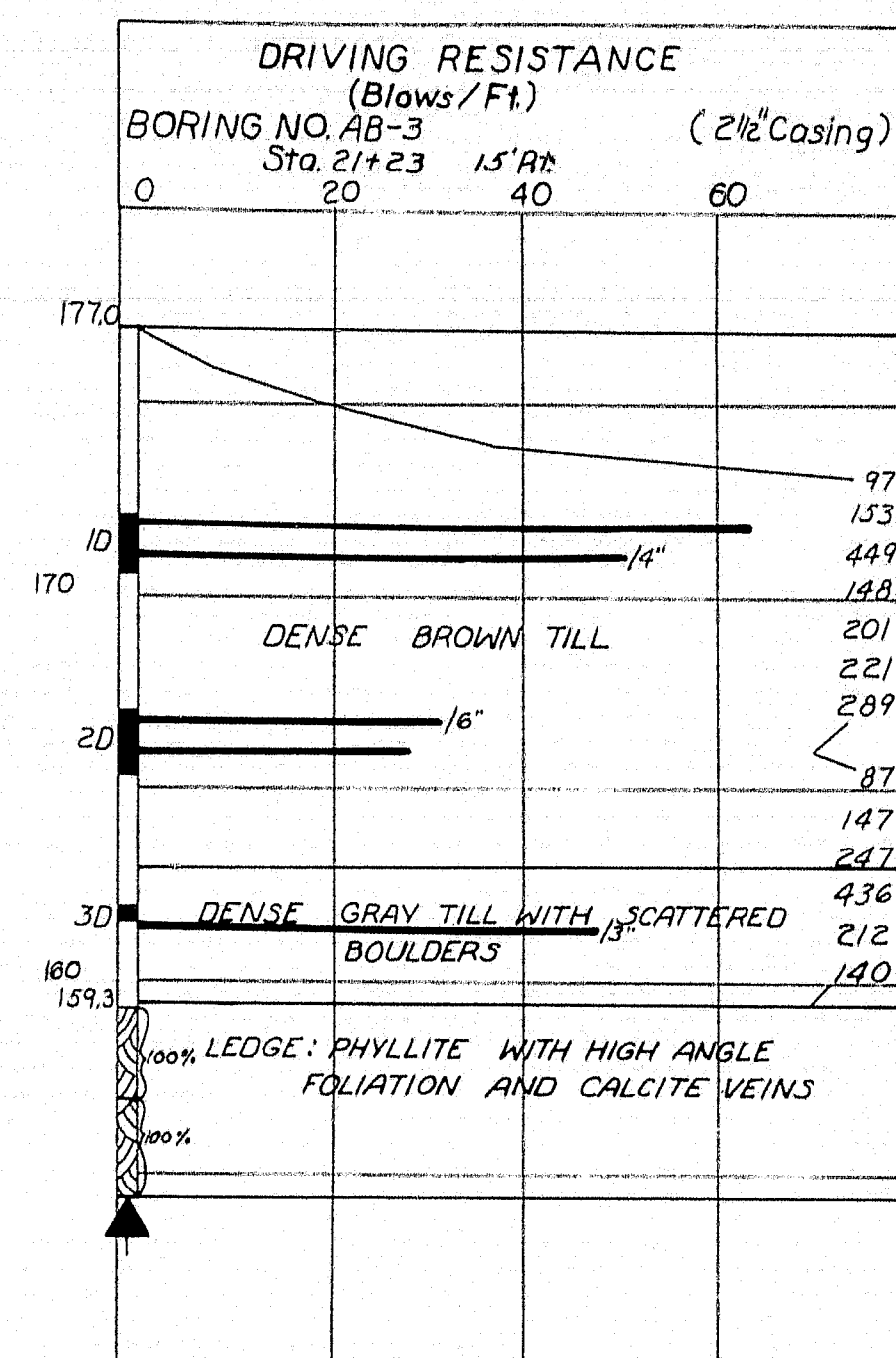
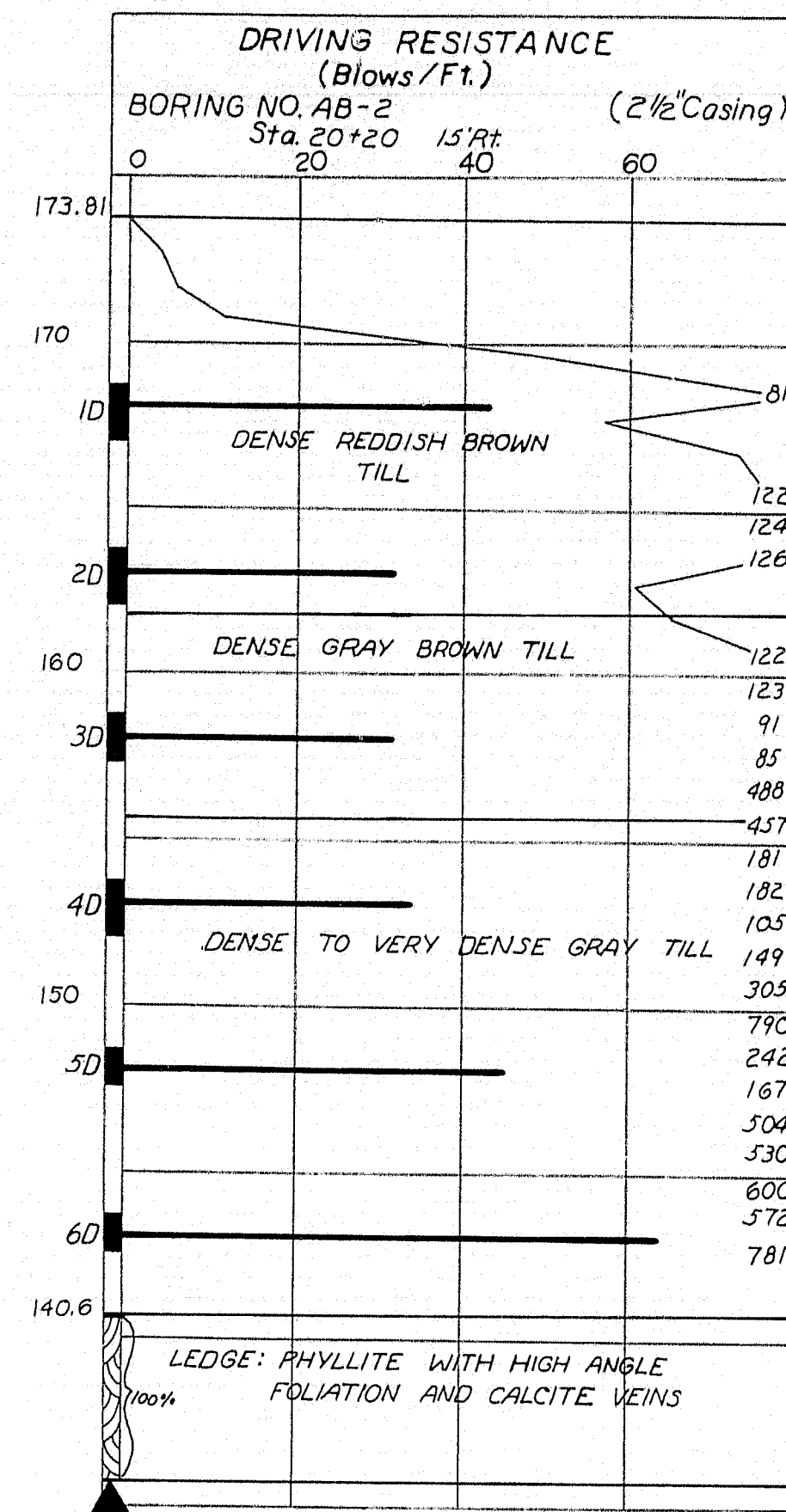
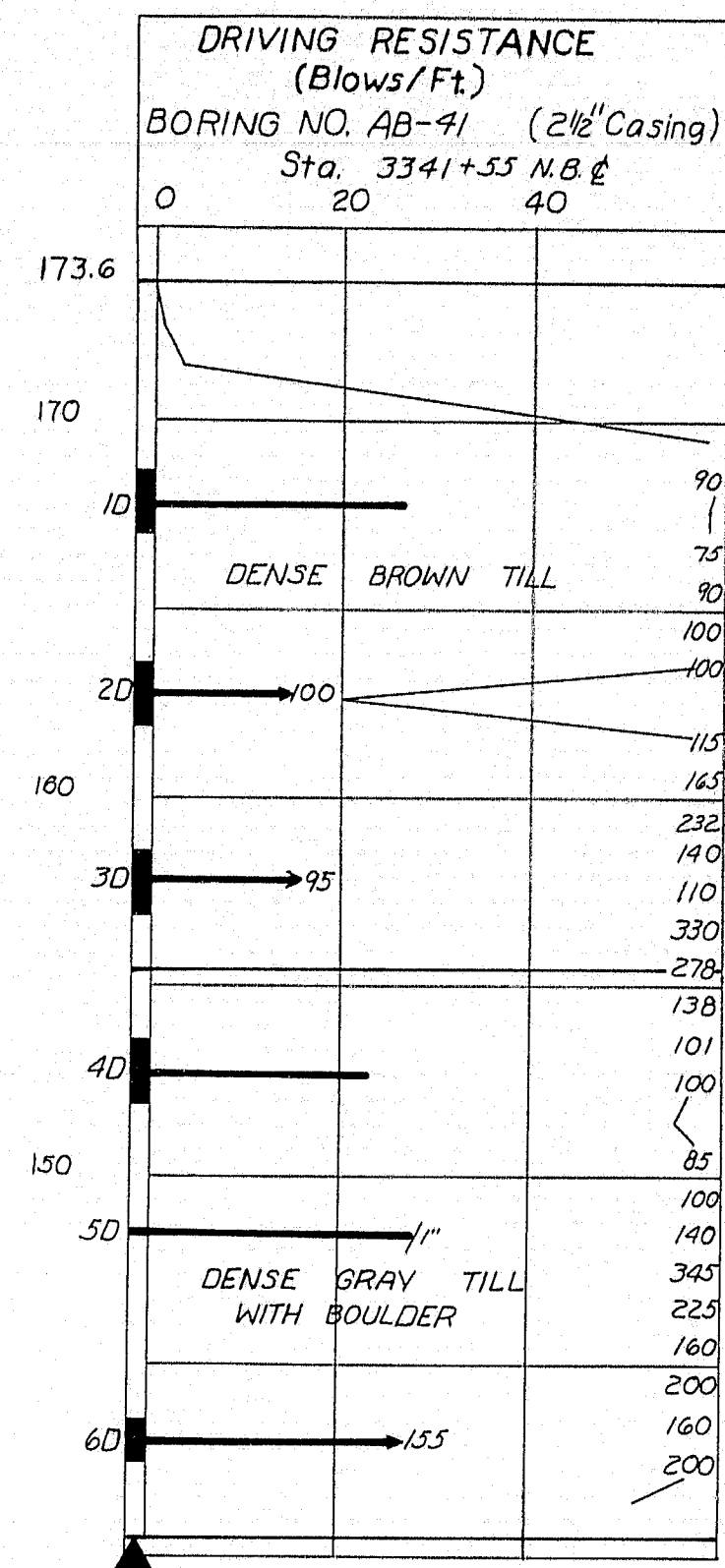
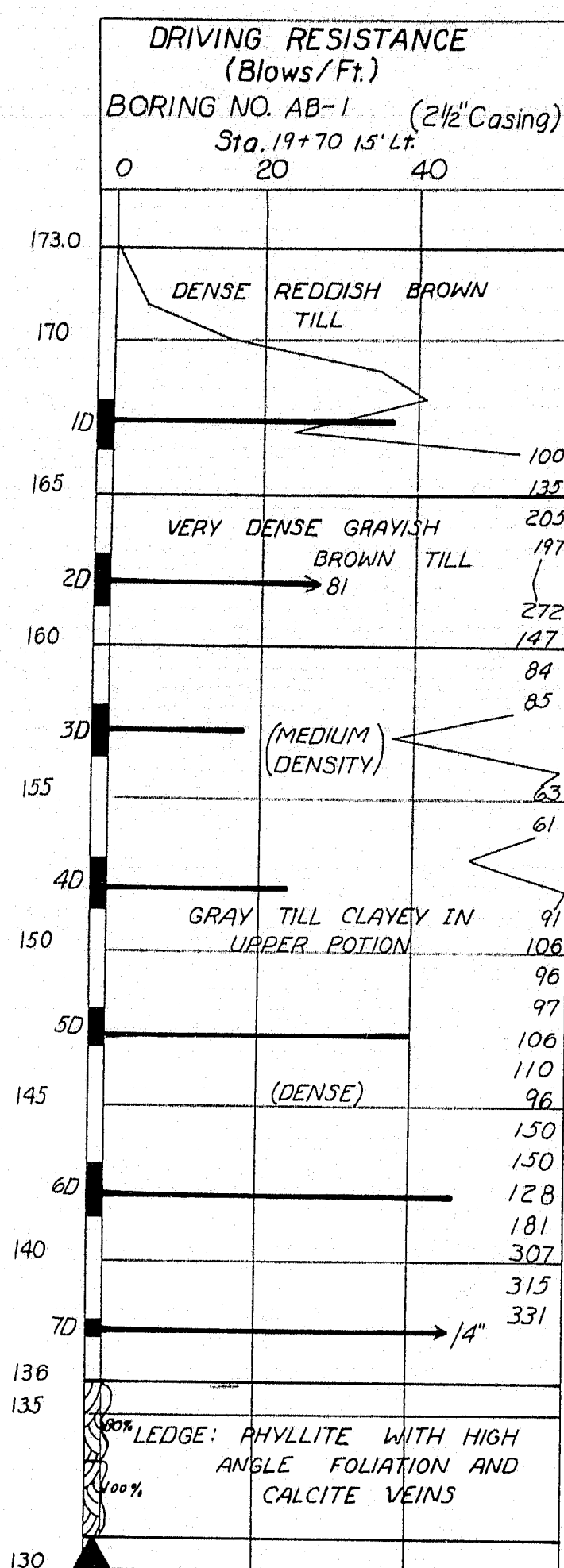
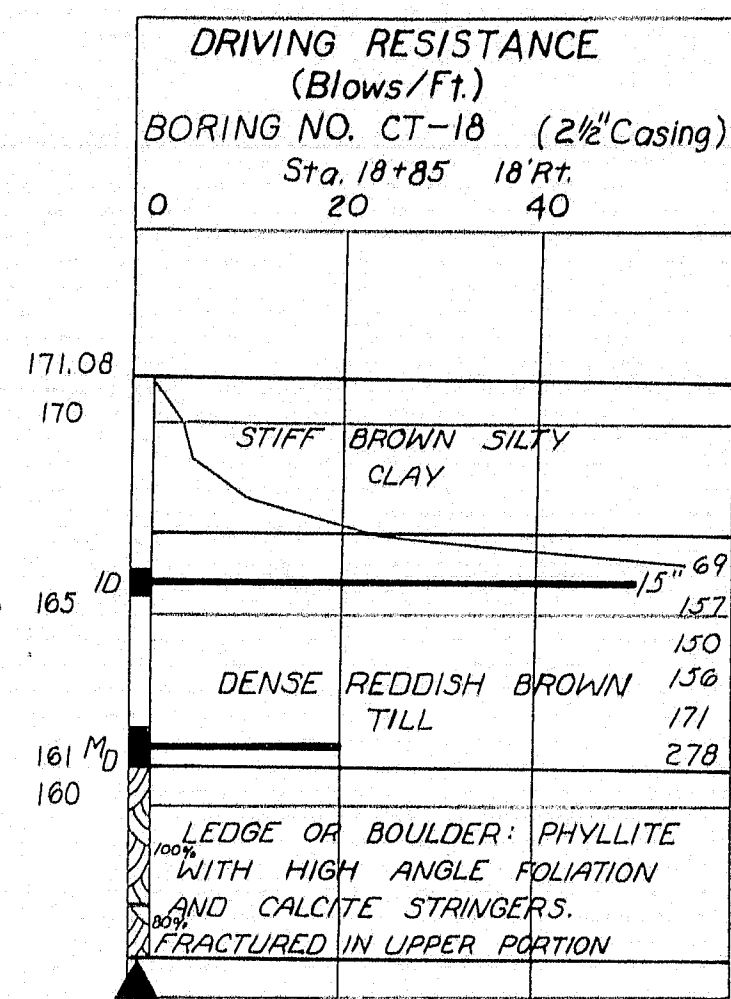
- BORING NOTES**
- ALL SAMPLES AND VANS ARE MADE AHEAD OF CASING
 - NUMBER OF BLOWS REQUIRED TO DRIVE EXTRA HEAVY CASING ONE FOOT WITH 400 ft. lbs. OF ENERGY PER BLOW
 - LOCATION OF SAMPLE OR SAMPLE ATTEMPT
 - NUMBER AND TYPE OF DRY SAMPLE
 - 1D 3/8" SAMPLER #1290'S
 - UNSUCCESSFUL SAMPLE ATTEMPT AND TYPE OF SAMPLER
 - NUMBER OF BLOWS REQUIRED TO DRIVE SPOON OR TUBING ONE FOOT WITH 350 ft. lbs. OF ENERGY PER BLOW
 - BOTTOM OF BORING (MAY NOT BE BOTTOM OF SOIL STRATA)
 - LOCATIONS CORED BY DIAMOND BIT AND PER CENT RECOVERY OF ROCK
 - NUMBER AND TYPE OF DRY SAMPLE
 - 1C 2" O.D. 16 ga. SEAMLESS TUBING
 - 1U 3 1/2" O.D. 16 ga. SEAMLESS TUBING
 - REFUSAL OF DRILL RODS OR CASING (MAY NOT BE LEDGE)

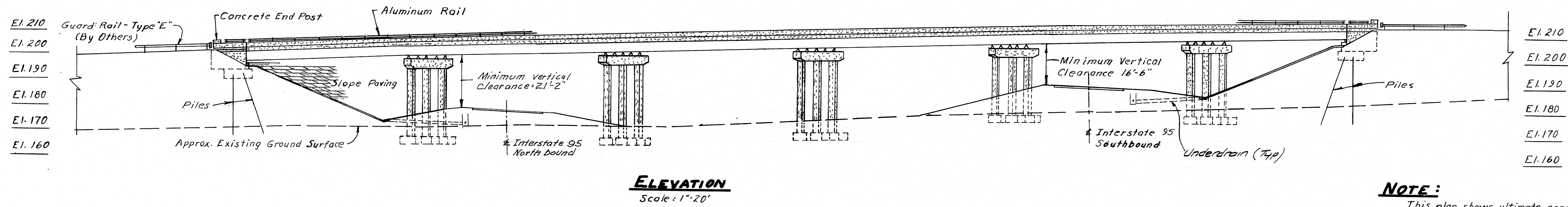
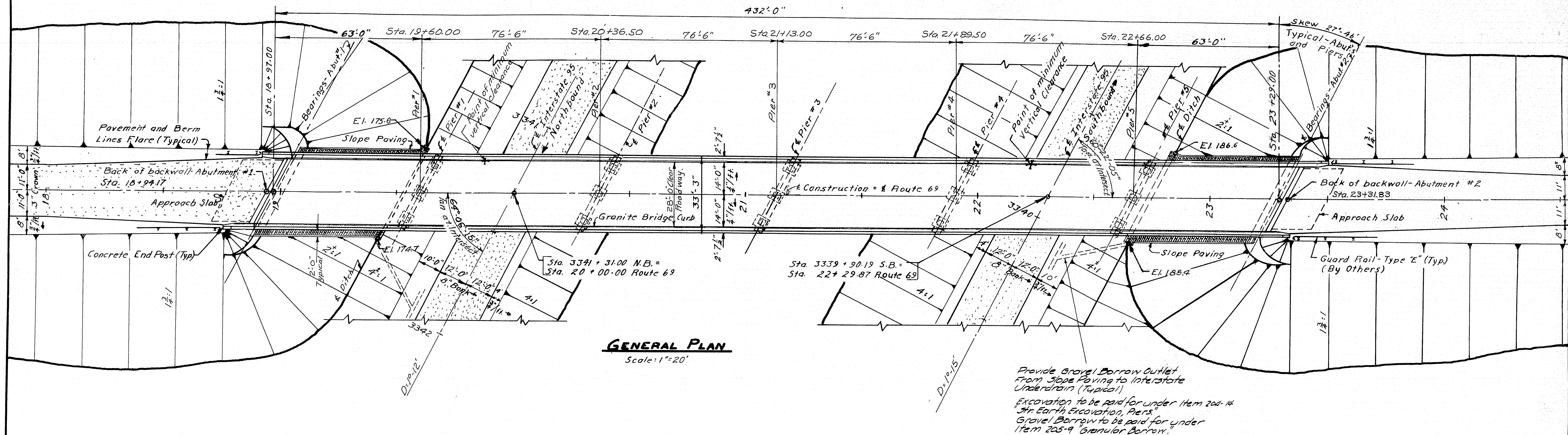
DESIGN-- SOILS DIVISION
TRACE-- BRIDGE NO.
CHECK-- SURVEY--
PLOT--

STATE HIGHWAY COMMISSION
BRIDGE DIVISION
ROUTE 69 BRIDGE
OVER
INTERSTATE HIGHWAY 95
IN THE TOWN OF
NEWBURY
PENOBSCOT COUNTY
FOUNDATION SURVEY
SHEET 3 OF 18 AUGUSTA, MAINE MARCH, 1962

VERT: 1"=5'
HORIZ: 1"=30'

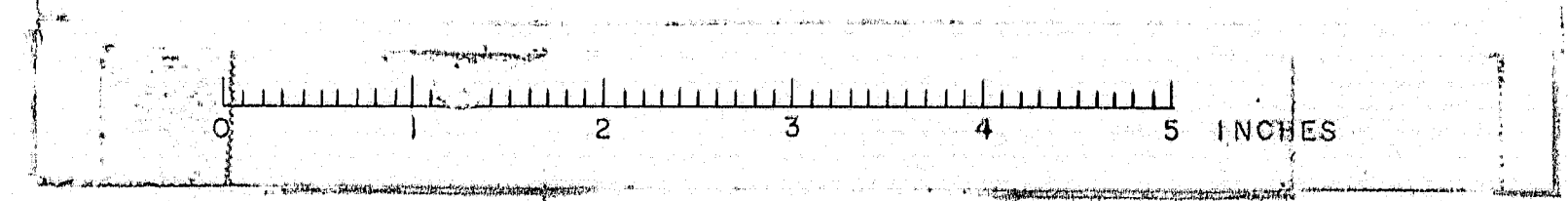
0 1 2 3 4 5 INCHES

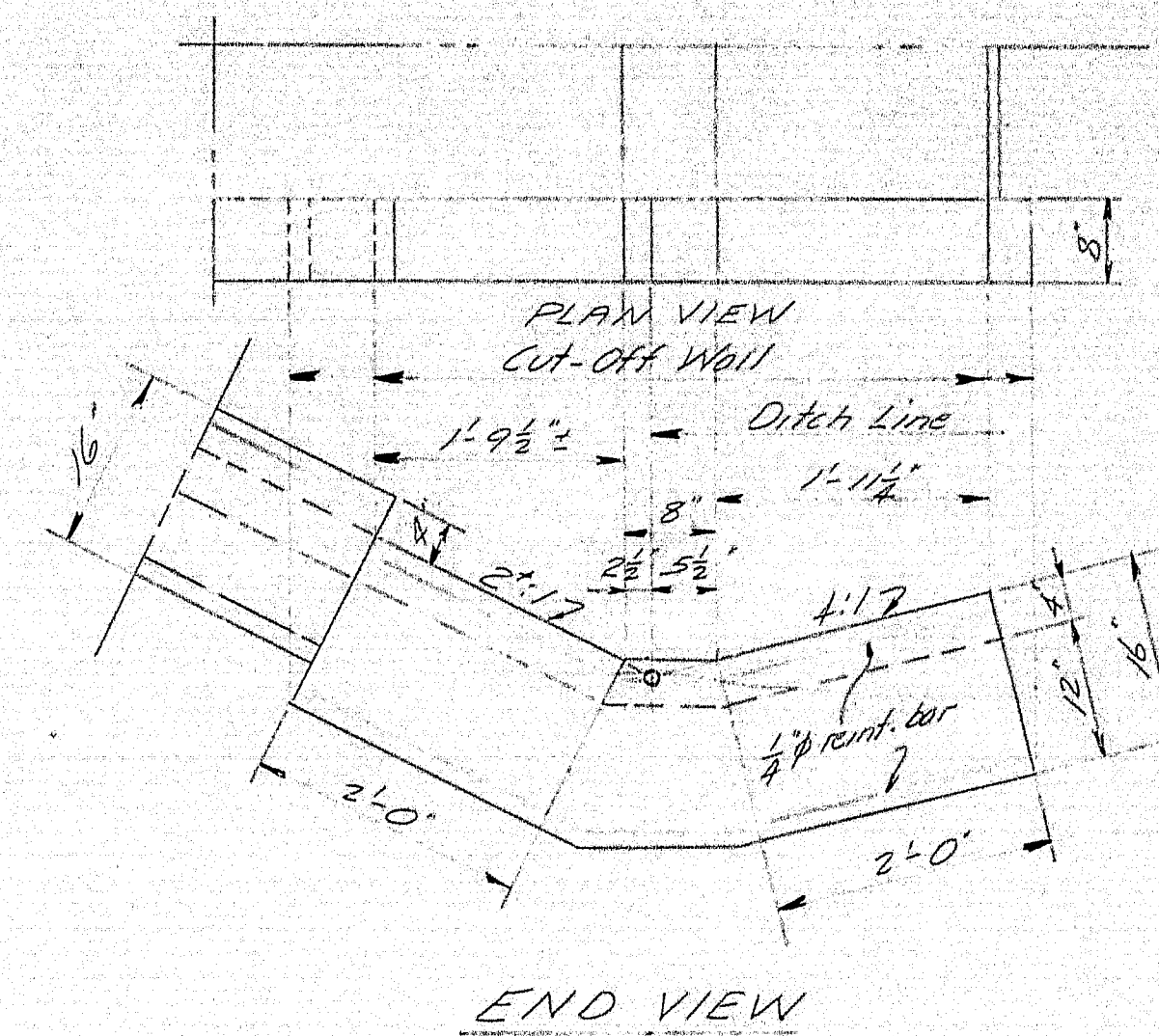
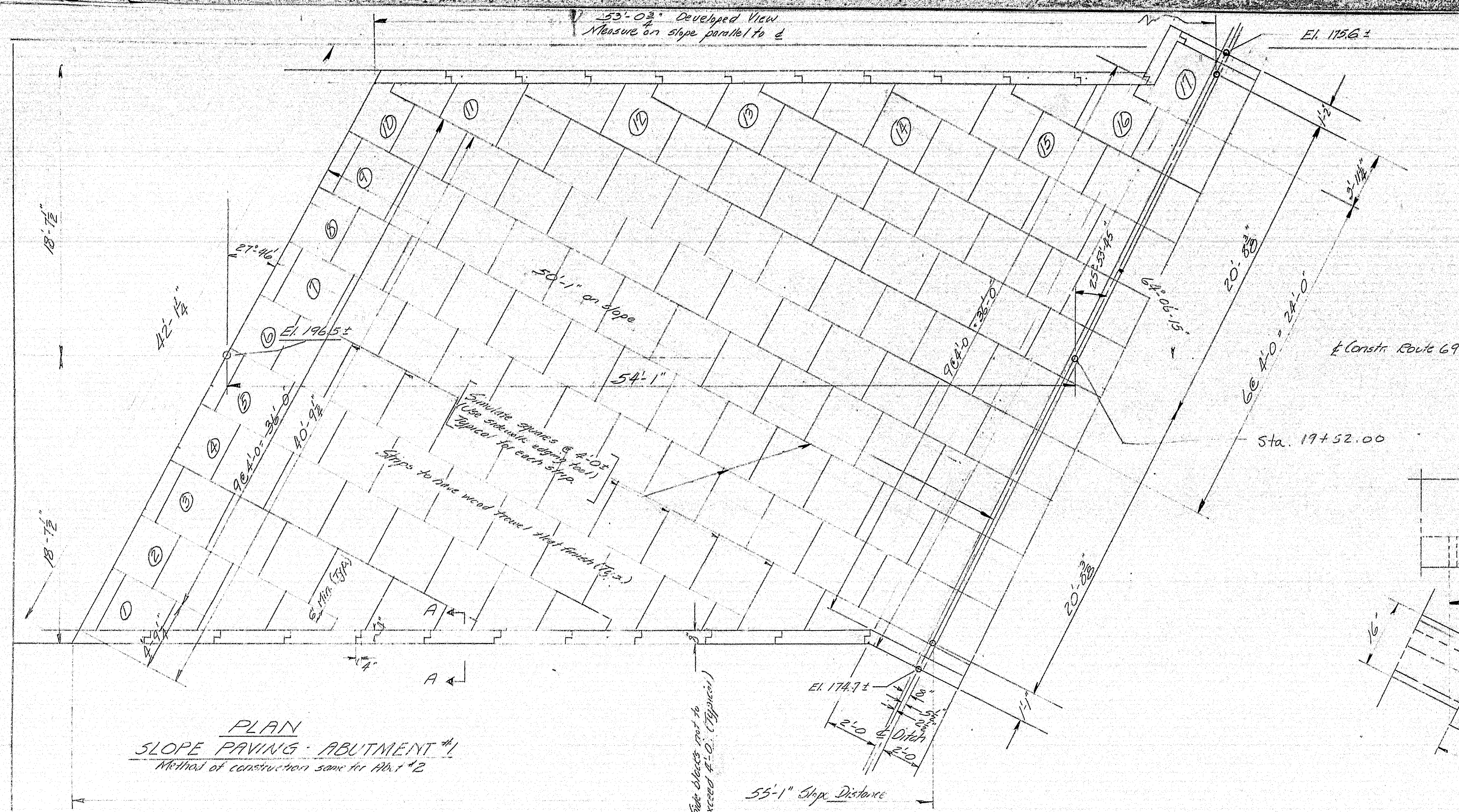




NOTE:
This plan shows ultimate construction. See Section Along Construction (sh. #6) for roadway work in this contract.

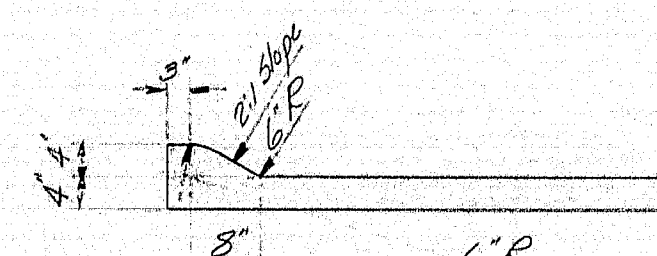
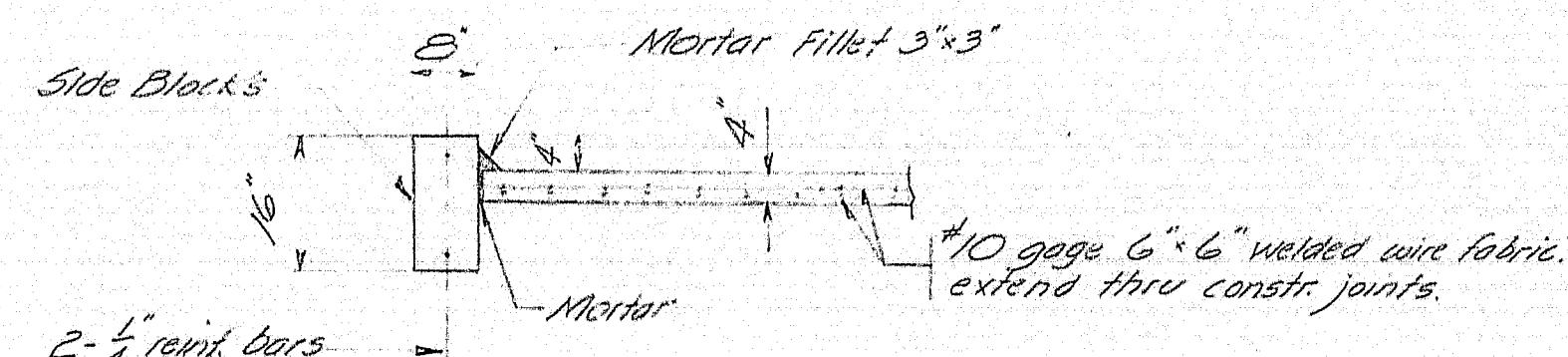
DESIGN - CDH	BRIDGE NO.
TRACE - B. M. D.	SURVEY -
CHECK -	PLAT -
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
ROUTE 69 BRIDGE OVER	
INTERSTATE HIGHWAY 95 IN THE TOWN OF NEWBURGH	
PENOBSCOT COUNTY	
GENERAL PLAN & ELEVATION SHEET 5 OF 18 AUGUST-MAINE-MARCH, 1962	





CAST IN PLACE DITCH CUT-OFF WALL
TYP BOTH ABUTS

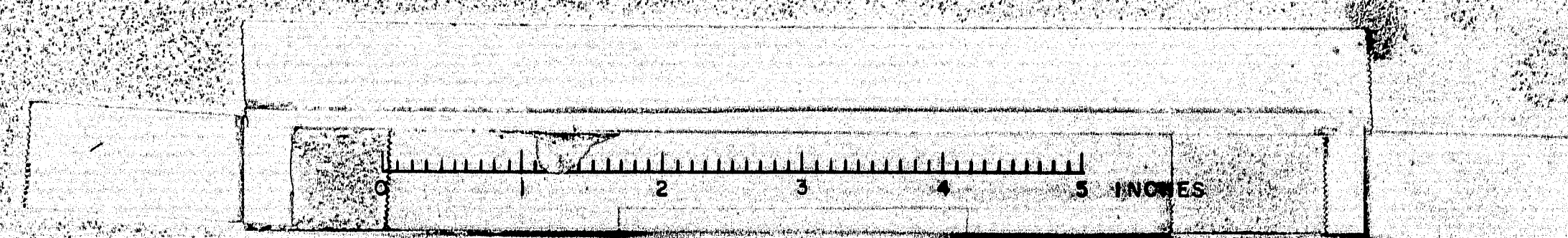
Note: Inside form may be left in place

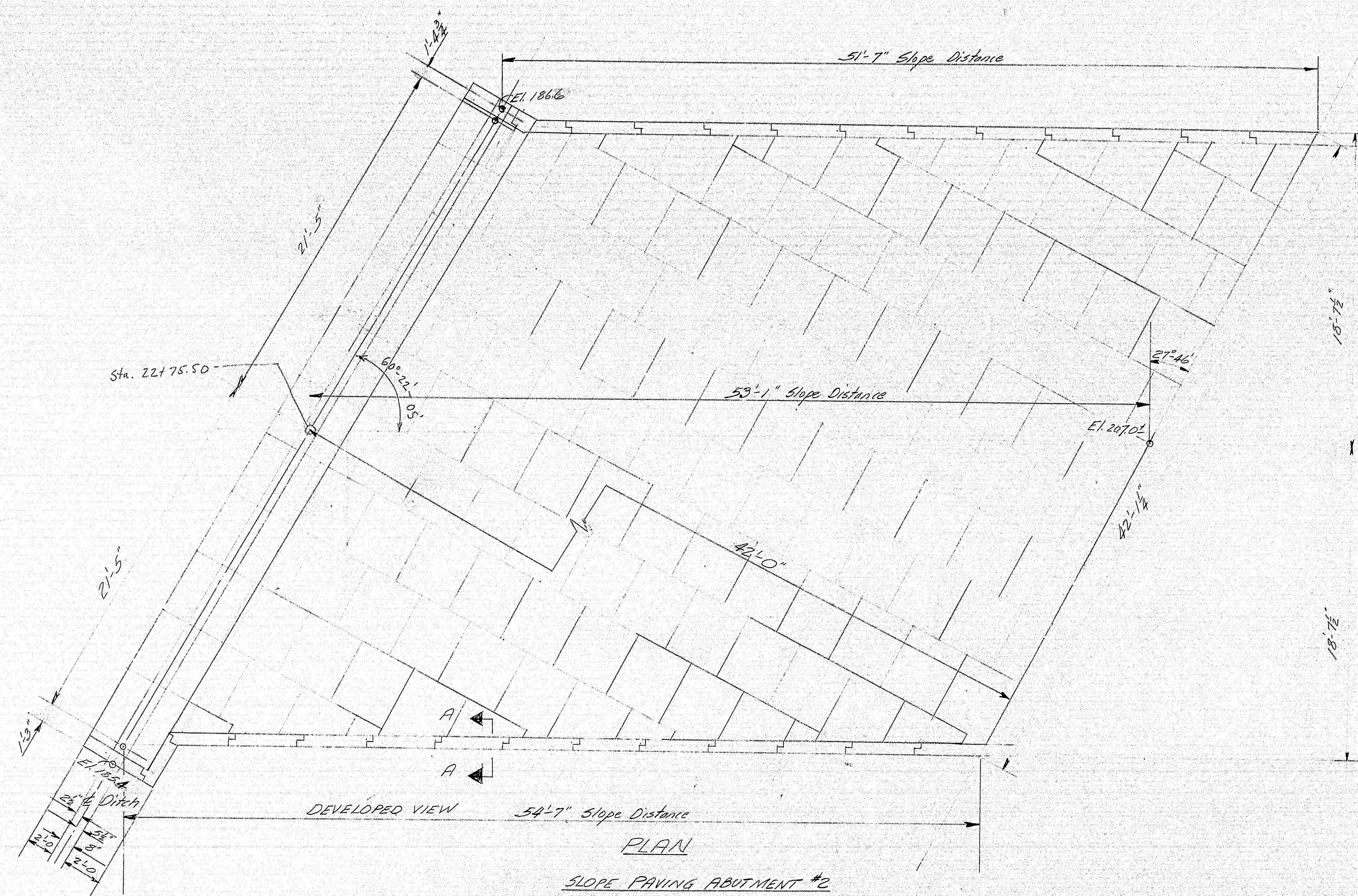


NOTE:
Welded wire fabric to extend thru joints.
Break bond at joints with a coat of asphalt paint.

Cast in place concrete strips to be placed in alternate bays 1-3-5-7-9-11-13-15-17. When forms are removed place 2-4-6-8-10-12-14-16. Side blocks are to be precast and placed after forms for concrete strips are removed.

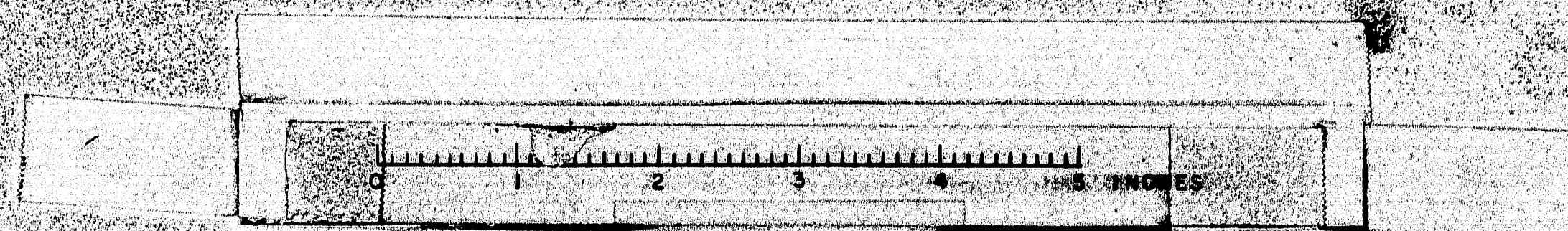
PLAN - MURSELL	
CHECK - BOWEN	
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
ROUTE 69 BRIDGE OVER	
INTERSTATE HIGHWAY 95 IN THE TOWN OF NEWBURGH	
PENOBSCOT COUNTY	
CAST IN PLACE SLOPE PAVING	
Sheet 1 of 2	AUGUSTA, MAINE AUG. 1962

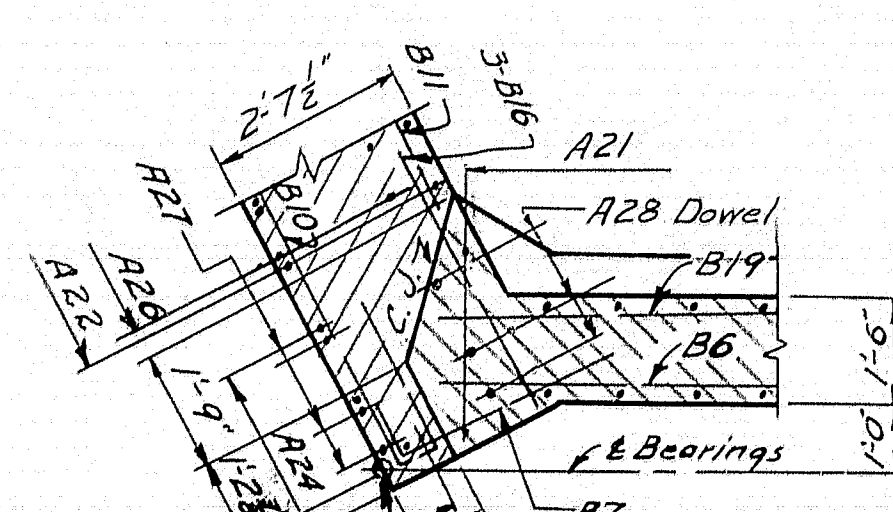




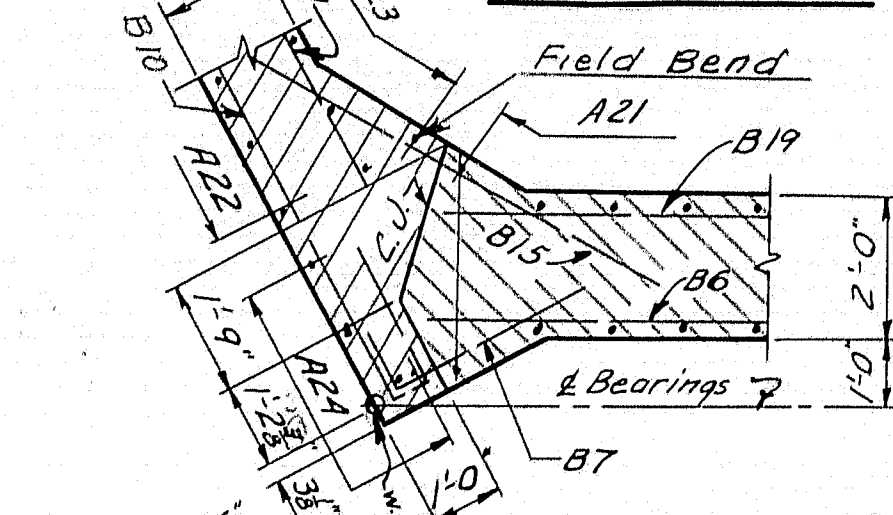
SLOPE PAVING ABUTMENT #2
Method of construction same as abutment #1

PLAN - MARRILL CHECKED - BAHG	STATE HIGHWAY COMMISSION BRIDGE DIVISION ROUTE 69 BRIDGE over INTERSTATE HIGHWAY 95 IN THE TOWN OF NEWBURGH PENOBSCOT COUNTY CAST IN PLACE SLOPE PAVING Sheet 2 of 2 AUGUSTA, MAINE APRIL 1962
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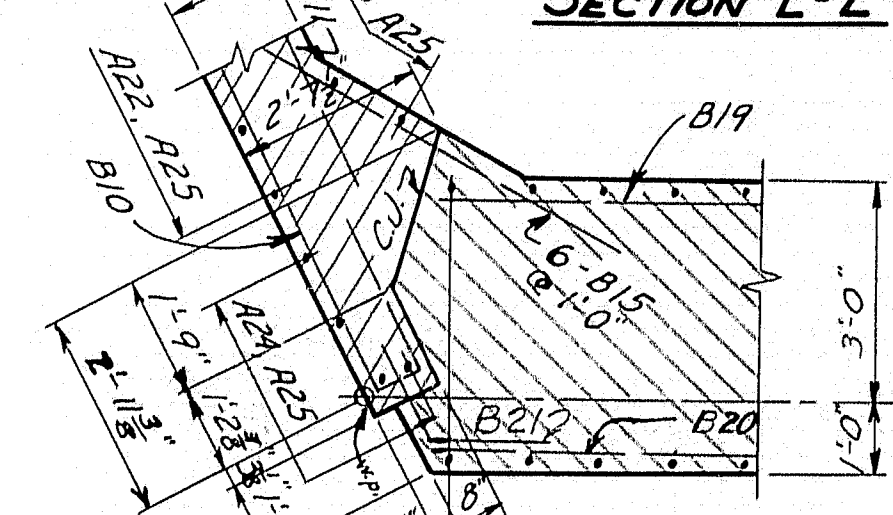




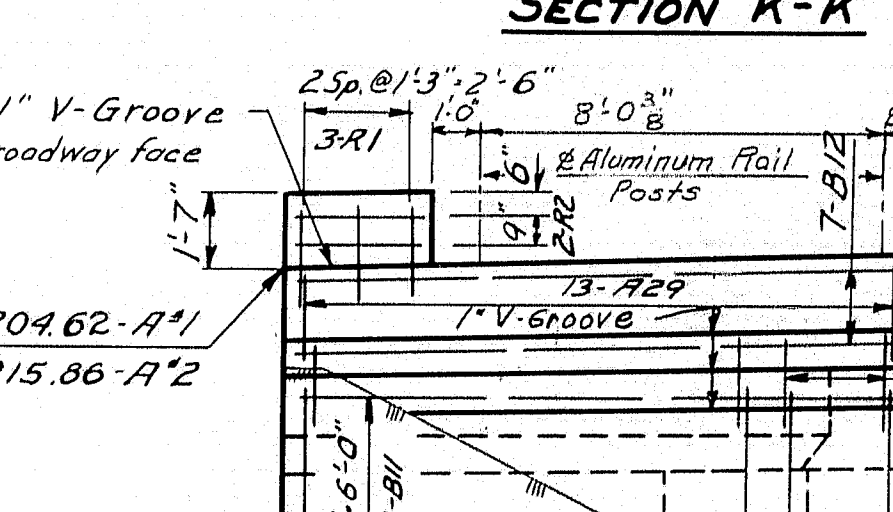
SECTION M-M



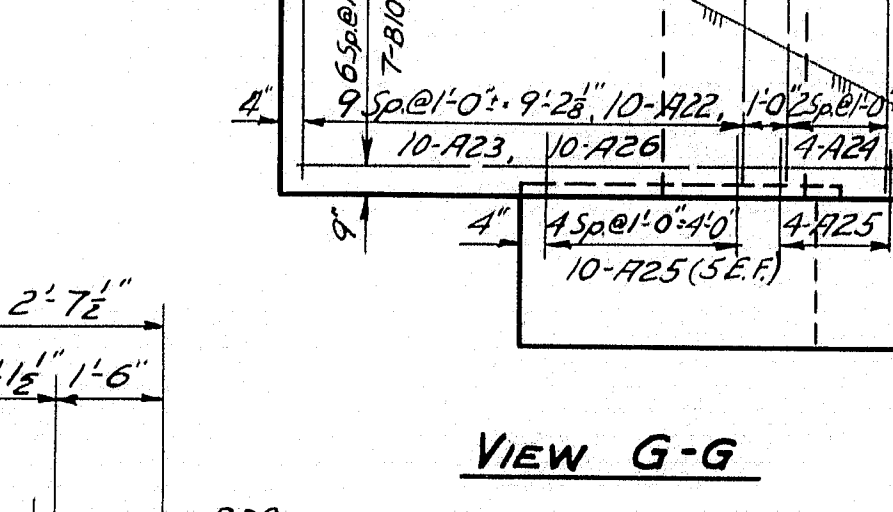
SECTION L-L



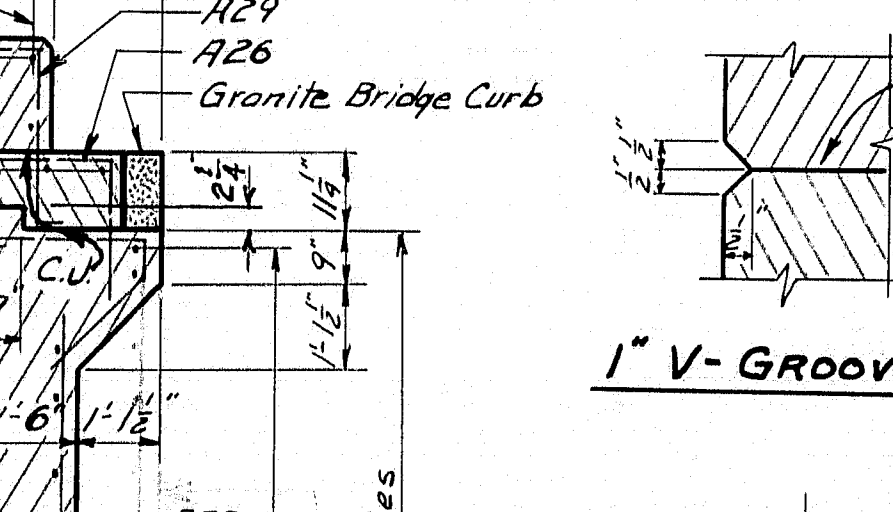
SECTION K-K



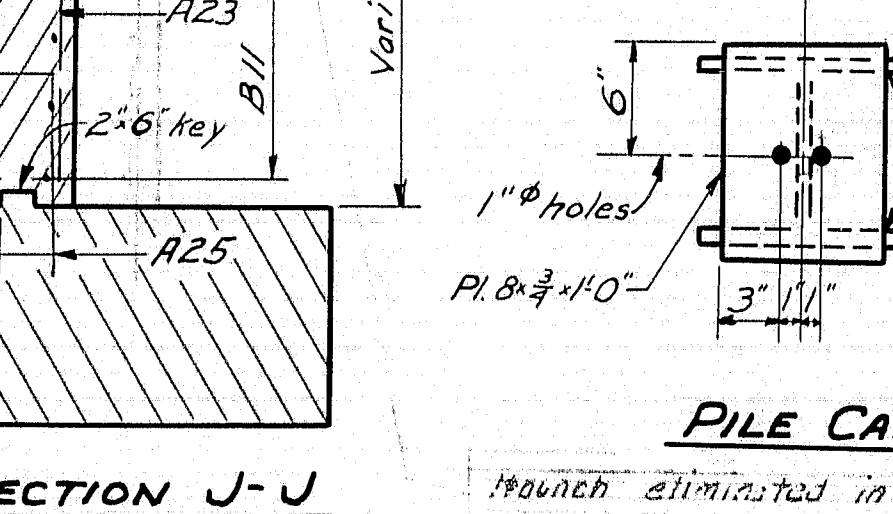
SECTION J-J



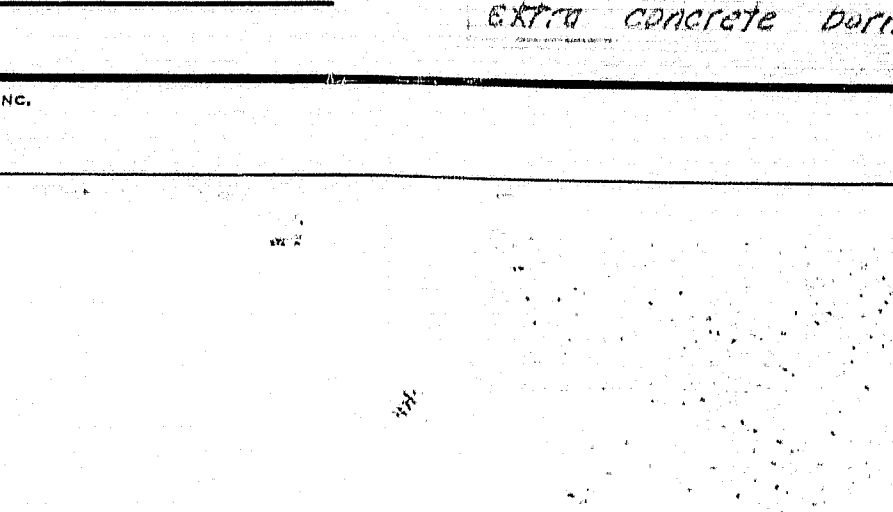
VIEW G-G



1" V-GROOVE



PILE CAP

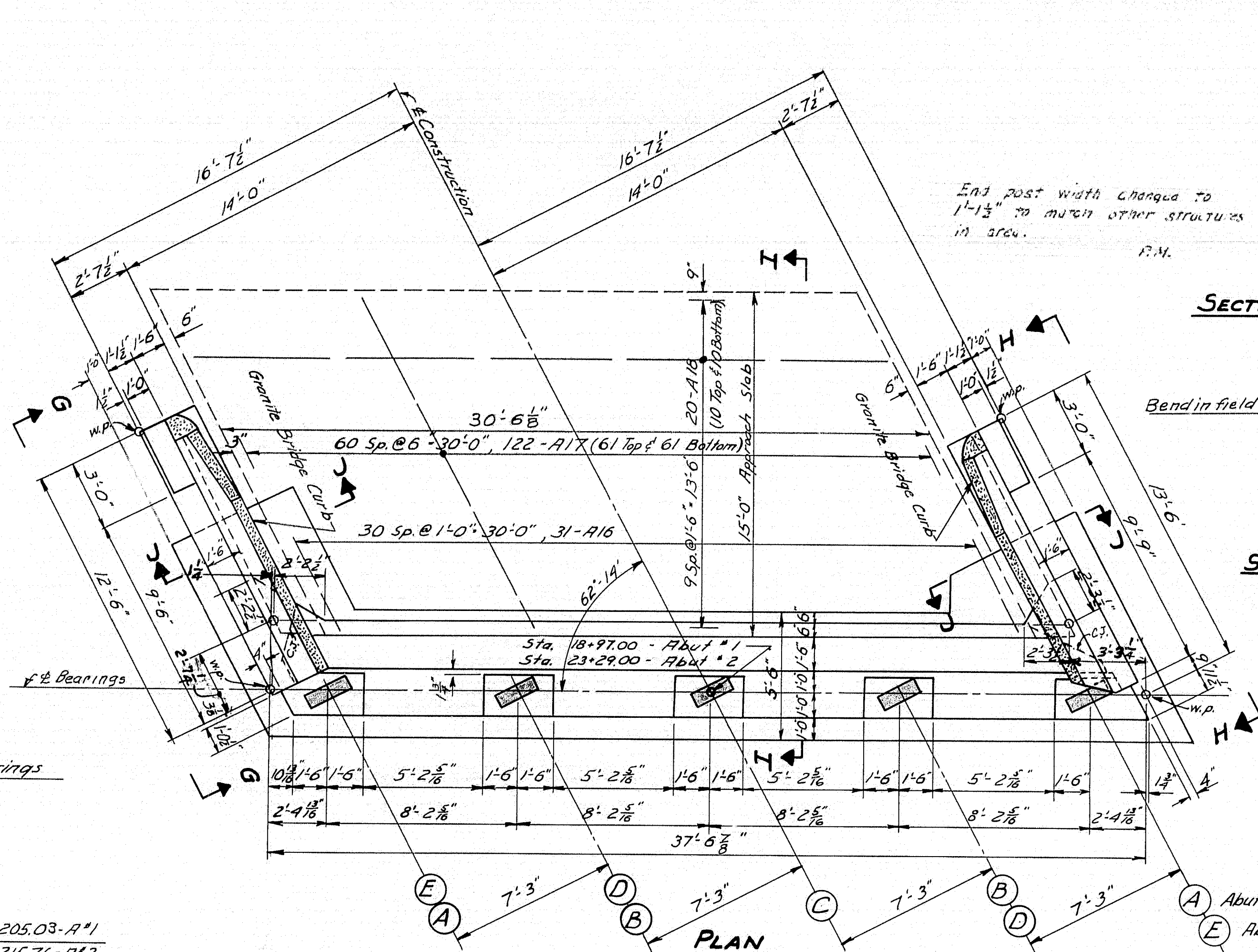


SECTION Q-Q

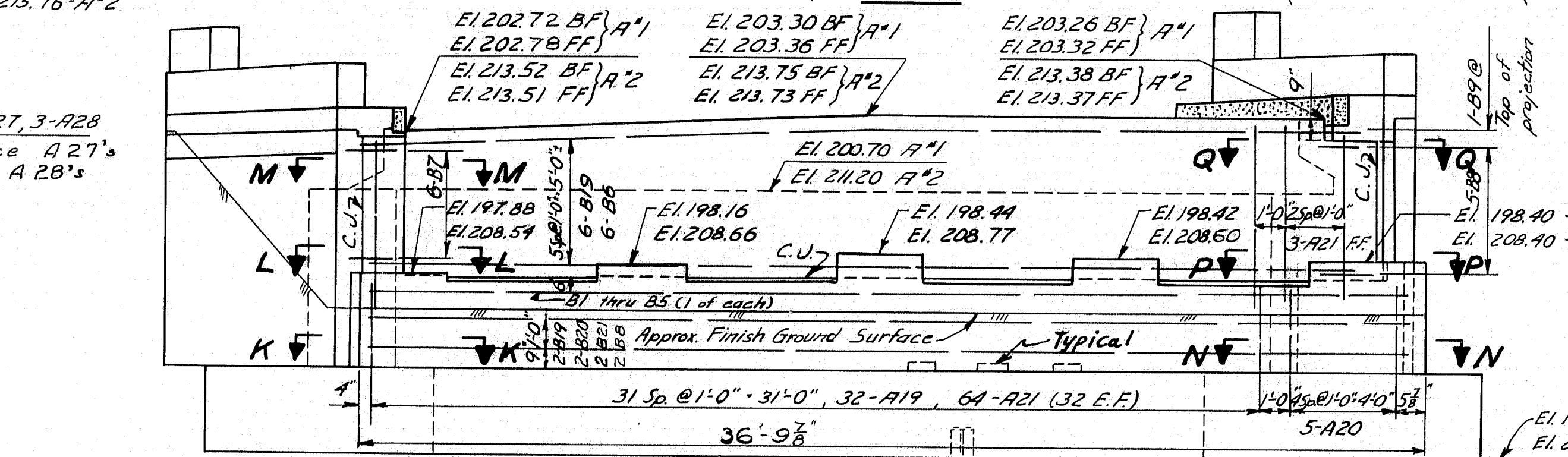
SECTION P-P

SECTION N-N

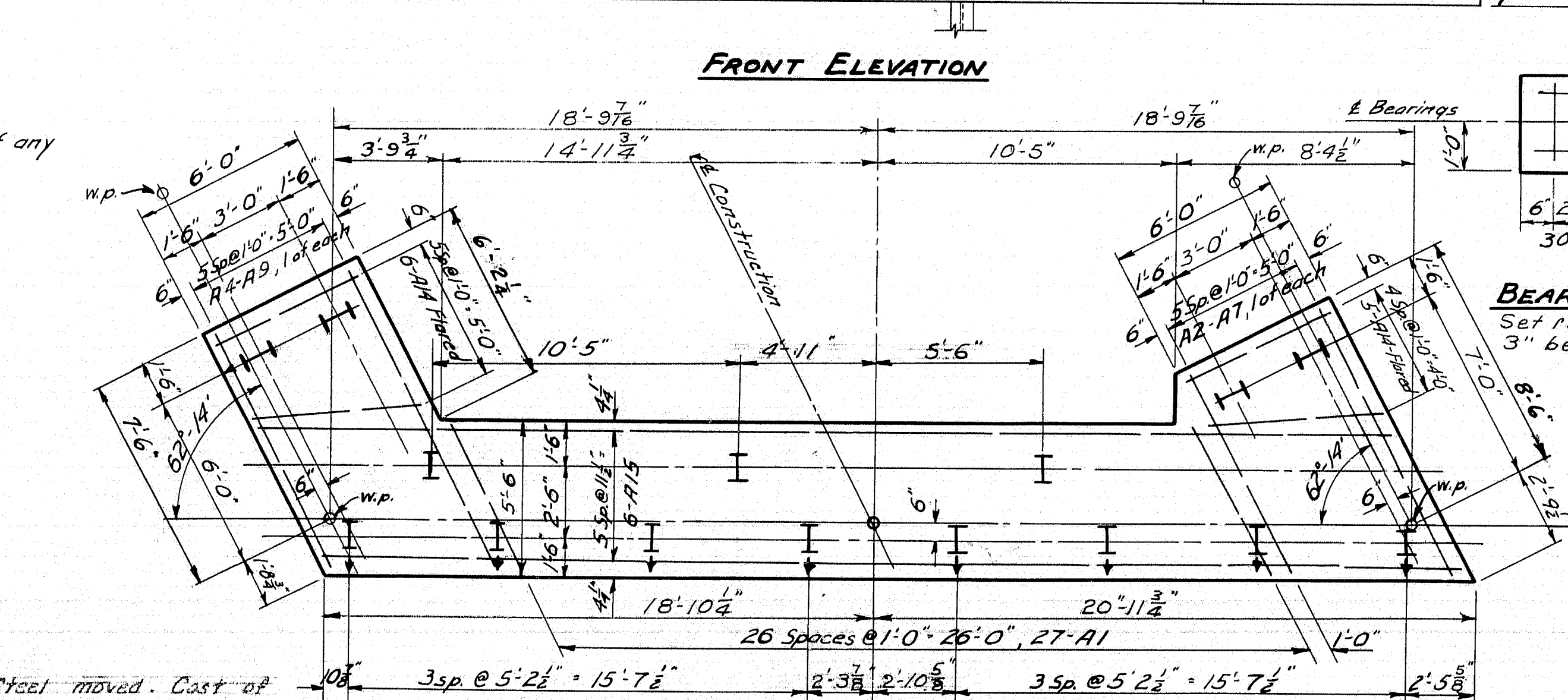
VIEW H-H



PLAN



FRONT ELEVATION



FOOTING AND PILE PLAN

BEARING PAD REINF.

Set reinforcing steel 3" below bridge seat.

PILE NOTES

1. All piles to be 10 BP 42, Steel H-piles.
2. Maximum Pile Load = 37 tons.
3. Piles marked thus \rightarrow to be battered 3" inch per foot in direction of arrow.
4. Piles to be driven to ledge or practical refusal to develop end bearing.
5. All piles are to be capped.
6. Estimated length of piles: Abutment #1 - 40 feet, Abutment #2 - 40 feet.

GENERAL NOTES

1. Place reinforcing steel in bridge seats to clear the anchor bolts.
2. Dress shaded bearing areas 1" larger all around than size of Masonry Plate.
3. Cover all unexposed joints (except of top of footing) on the back with two layers of heavy roofing 10' wide. Coat the surface of concrete and the back of each layer as applied with a suitable grade of roofing cement. The area to be covered shall be recessed 1/2 inch.
4. Aluminum Rail is not shown. For Details see sheet 17.
5. Concrete in approach slabs to be paid for under Item 701-33, Portland Cement Concrete, Abutments & Retaining Walls.
6. For Roadway Work and Slope Paving at abutments refer to sheet #6.
7. W.P. = Working Point, C.U. = Construction Joint, B.F. = Back Face, R.F. = Front Face, E.F. = Each Face, A'1 = Abutment #1, A'2 = Abutment #2.
8. For Details of Granite Bridge Curb see sh. #14.
9. Coat face of backwalls, bridge seats, & face of abutments down to top of footings with Epoxy Resin Surface Sealant.

DESIGN - CDH	BRIDGE NO.
TRACE - JH	SURVEY - 117
CHECK - CDH	PILOT -
STATE HIGHWAY COMMISSION	
BRIDGE DIVISION	
ROUTE 69 BRIDGE	
OVER	
INTERSTATE HIGHWAY 95	
IN THE TOWN OF	
NEWBURN	
PENOBSCOT COUNTY	
ABUTMENTS	
SHEET 7 OF 18	AUGUSTA, MAINE, MARCH, 1962

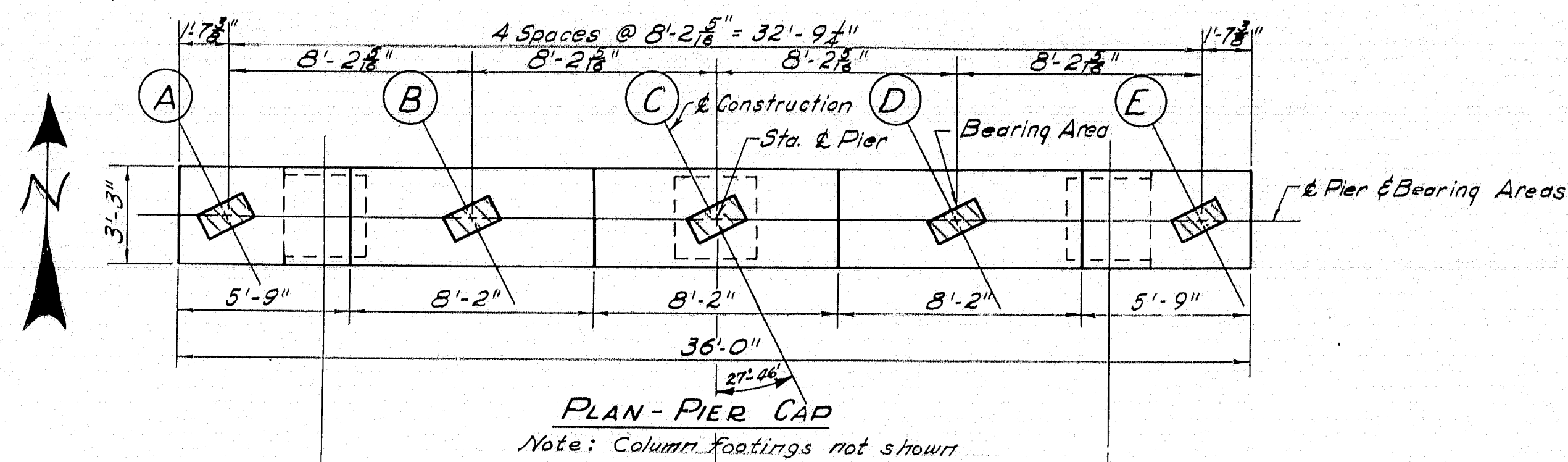
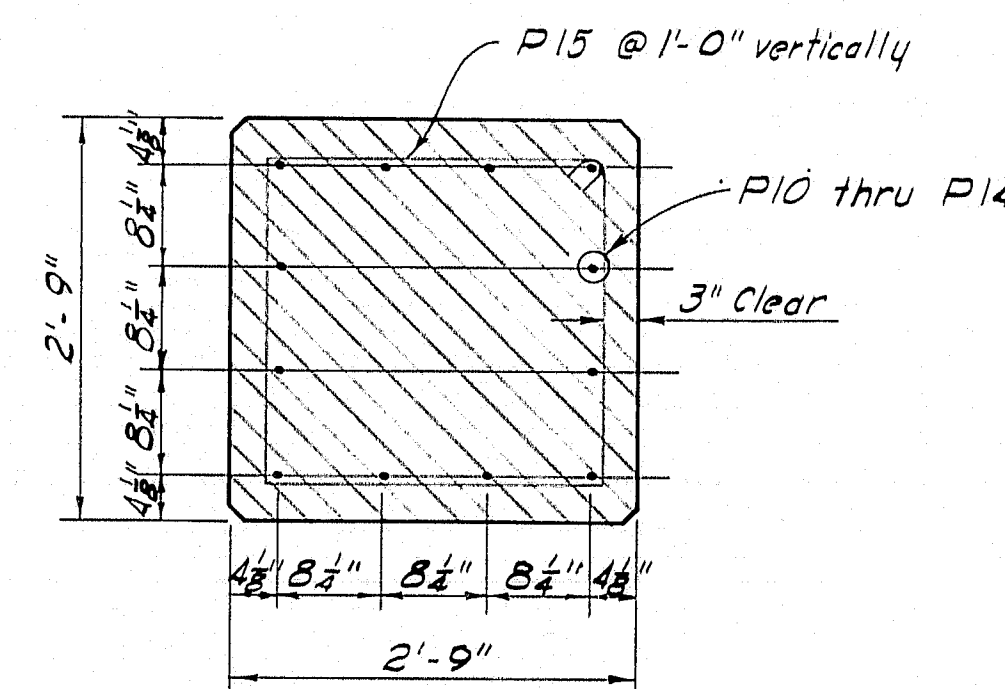
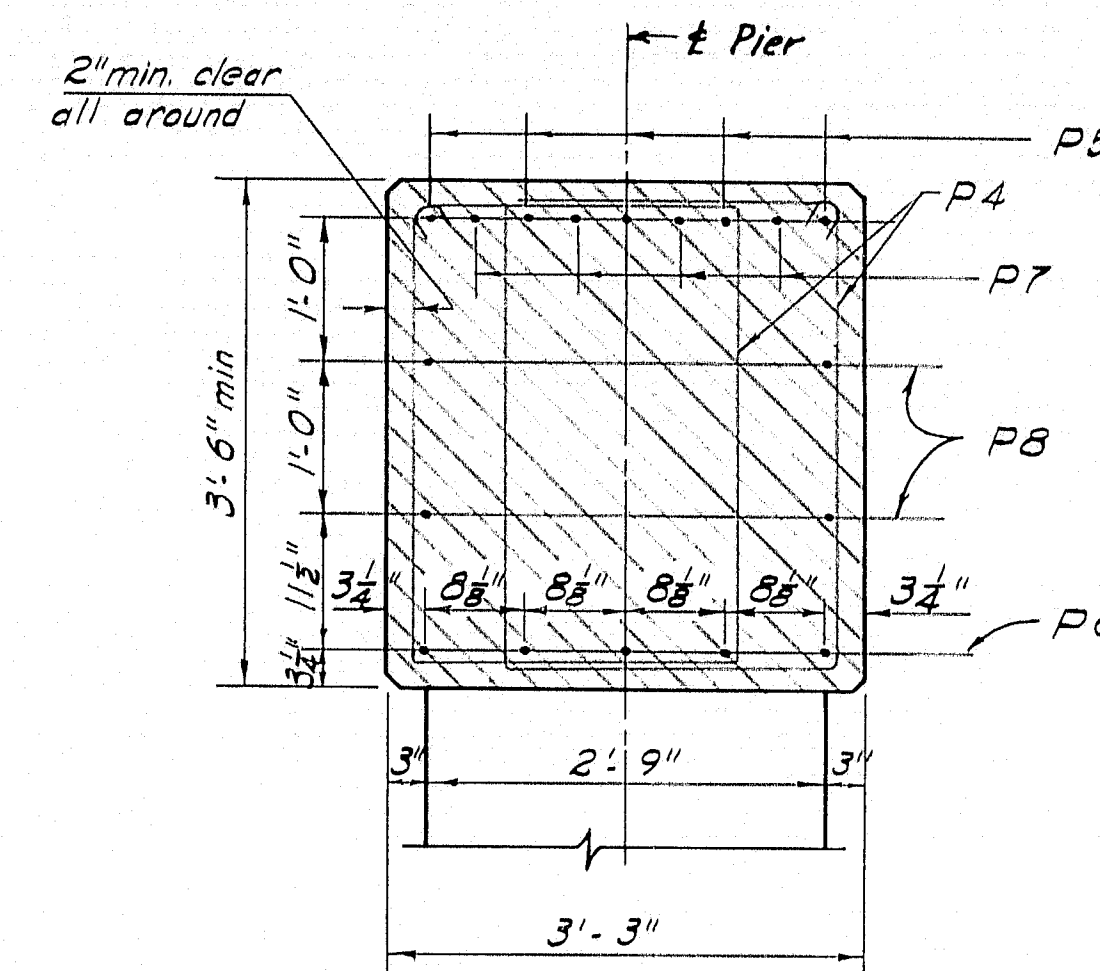
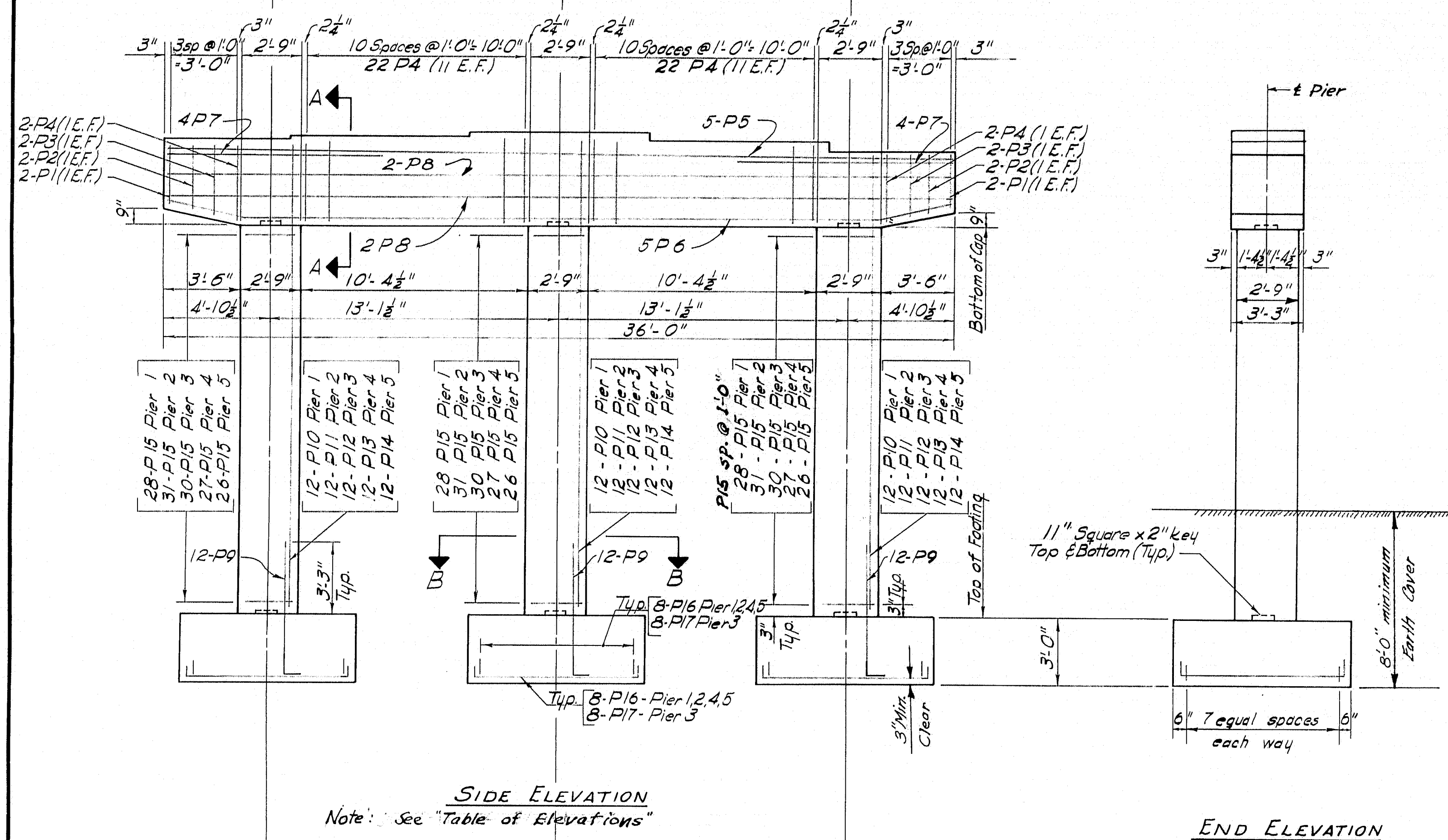
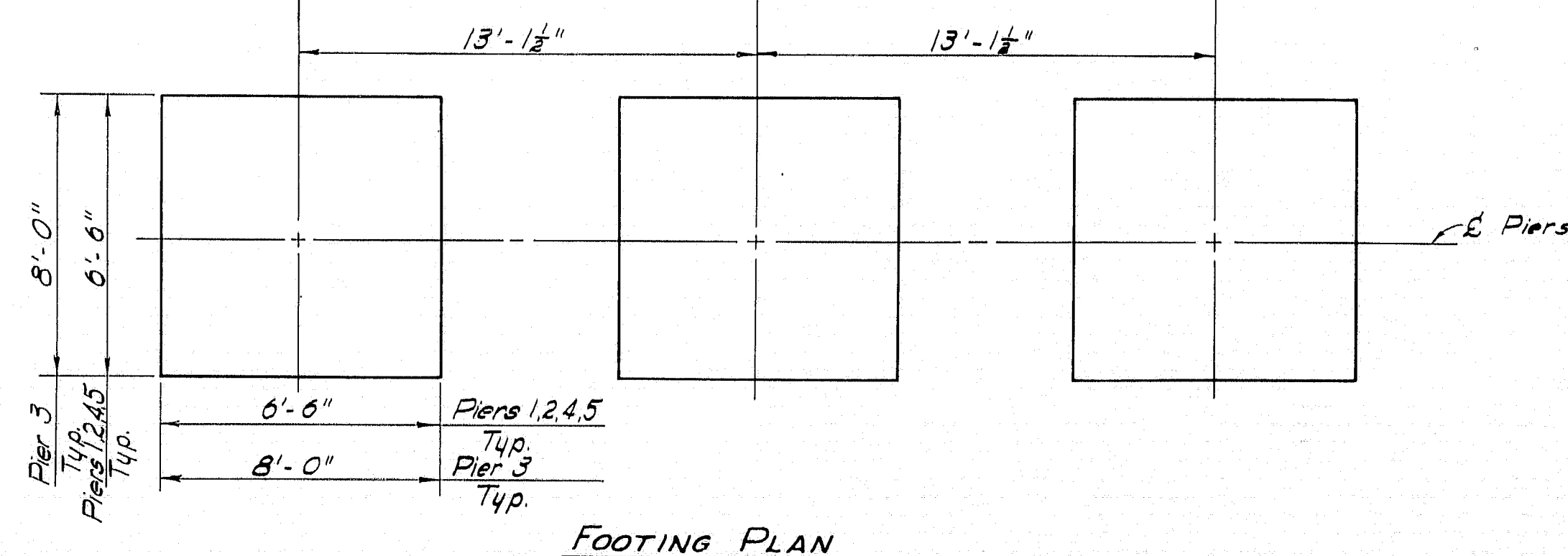


TABLE OF ELEVATIONS					
BEARING AREA	PIER 1	PIER 2	PIER 3	PIER 4	PIER 5
A	200.45	202.88	205.10	206.53	207.75
B	200.47	202.92	205.16	206.61	207.85
C	200.49	202.96	205.22	206.69	207.95
D	200.21	202.69	204.97	206.47	207.75
E	199.93	202.43	204.73	206.24	207.54
Bottom of Cap	196.40	198.90	201.20	202.70	204.00
Bottom of Footing	166.00	166.00	169.00	173.00	176.00
Sta. E. Pier	19+60.00	20+36.50	21+13.00	21+59.50	22+66.00

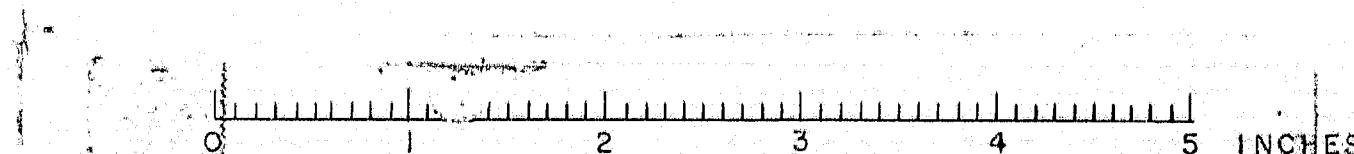


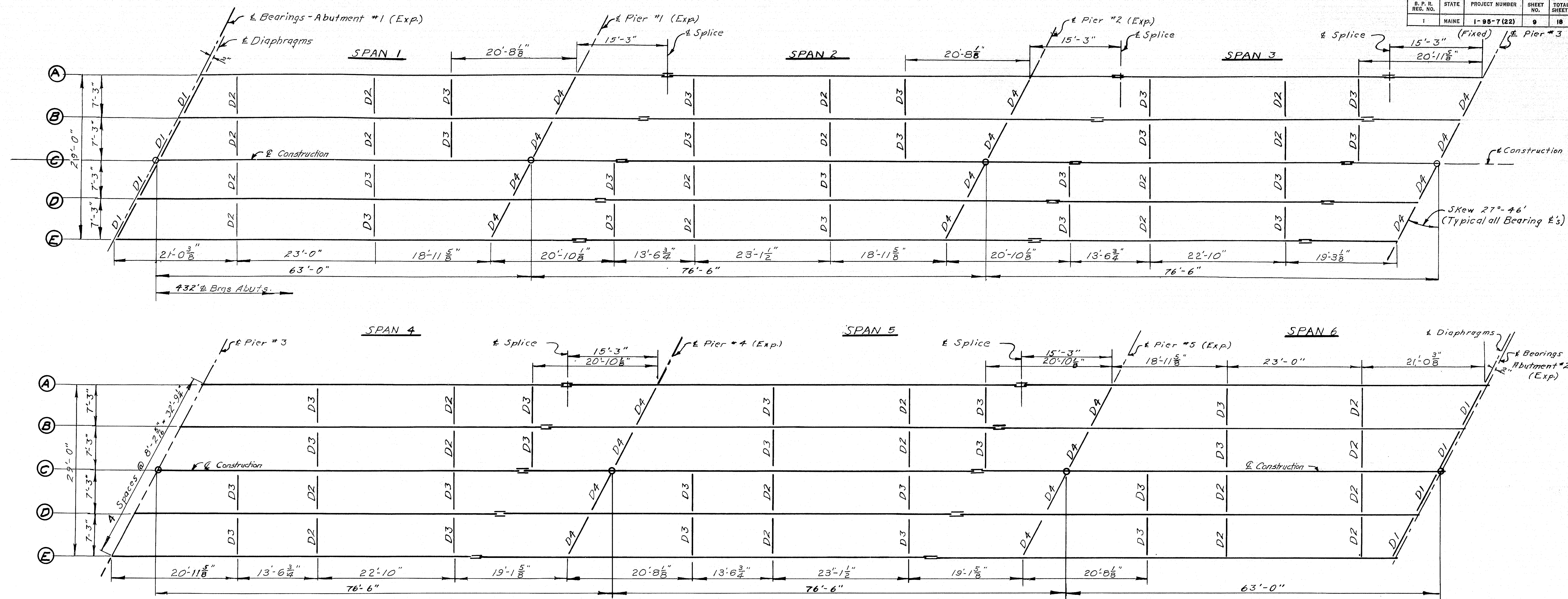
GENERAL NOTES

1. Position reinforcing steel in pier cap, under bearings, to clear swaged anchor bolts.
2. Chamfer all exposed edges of concrete $\frac{3}{4}$ inch.
3. Dress the shaded bearing areas on the bridge seats 1" larger all around than masonry plates to exact elevations shown in "Table of Elevations".
4. Maximum Footing Pressure: 6.5 tons/sq.ft. - Pier 3.
6.0 tons/sq.ft. - Piers 1, 2, 4, 5.
5. E.F. means Each Face.



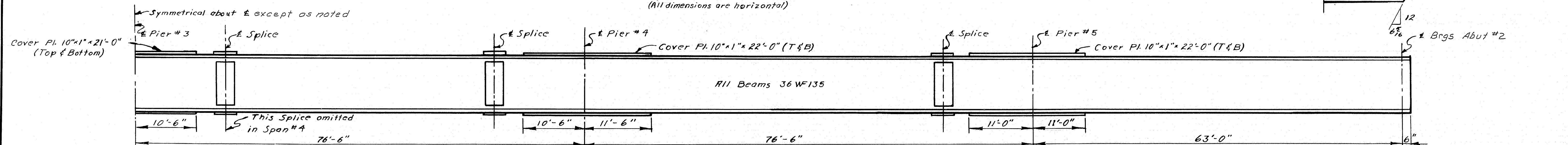
DESIGN - C.D.H. DET - J.H.W.	BRIDGE NO.
TRACE - S.B. JRM.	SURVEY -
CHECK -	PILOT -
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
ROUTE 69 BRIDGE OVER	
INTERSTATE HIGHWAY 95 IN THE TOWN OF NEWBURGH	
PENOBSCOT COUNTY PIERS	
SHEET 8 OF 18 AUGUSTA, MAINE MARCH, 1962	





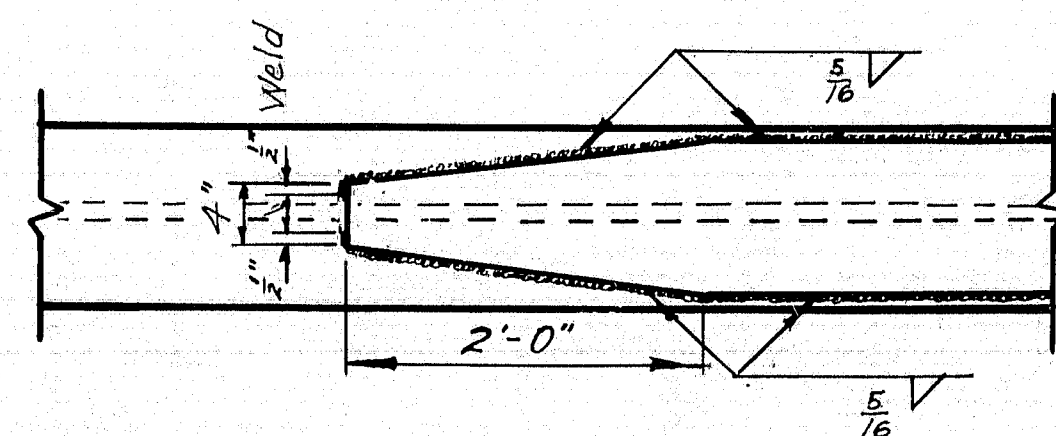
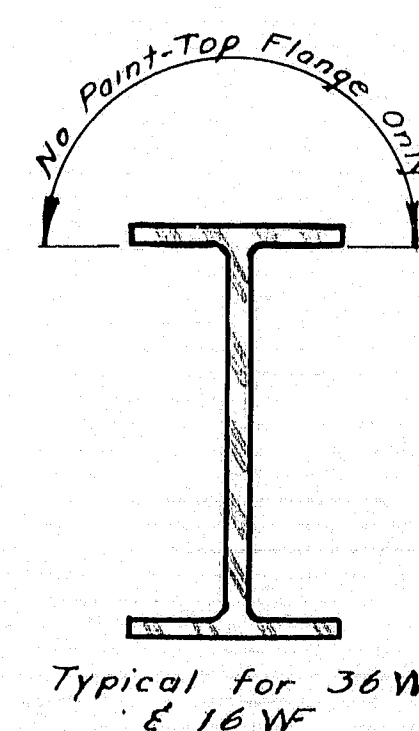
FRAMING PLAN

(All dimensions are horizontal)



ELEVATION OF STRINGER

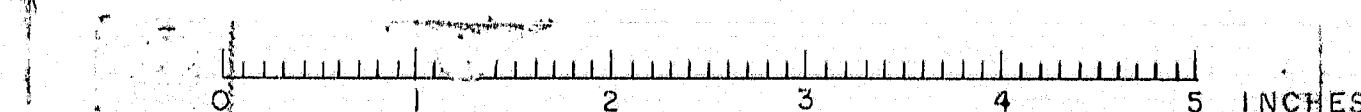
(All dimensions are horizontal)

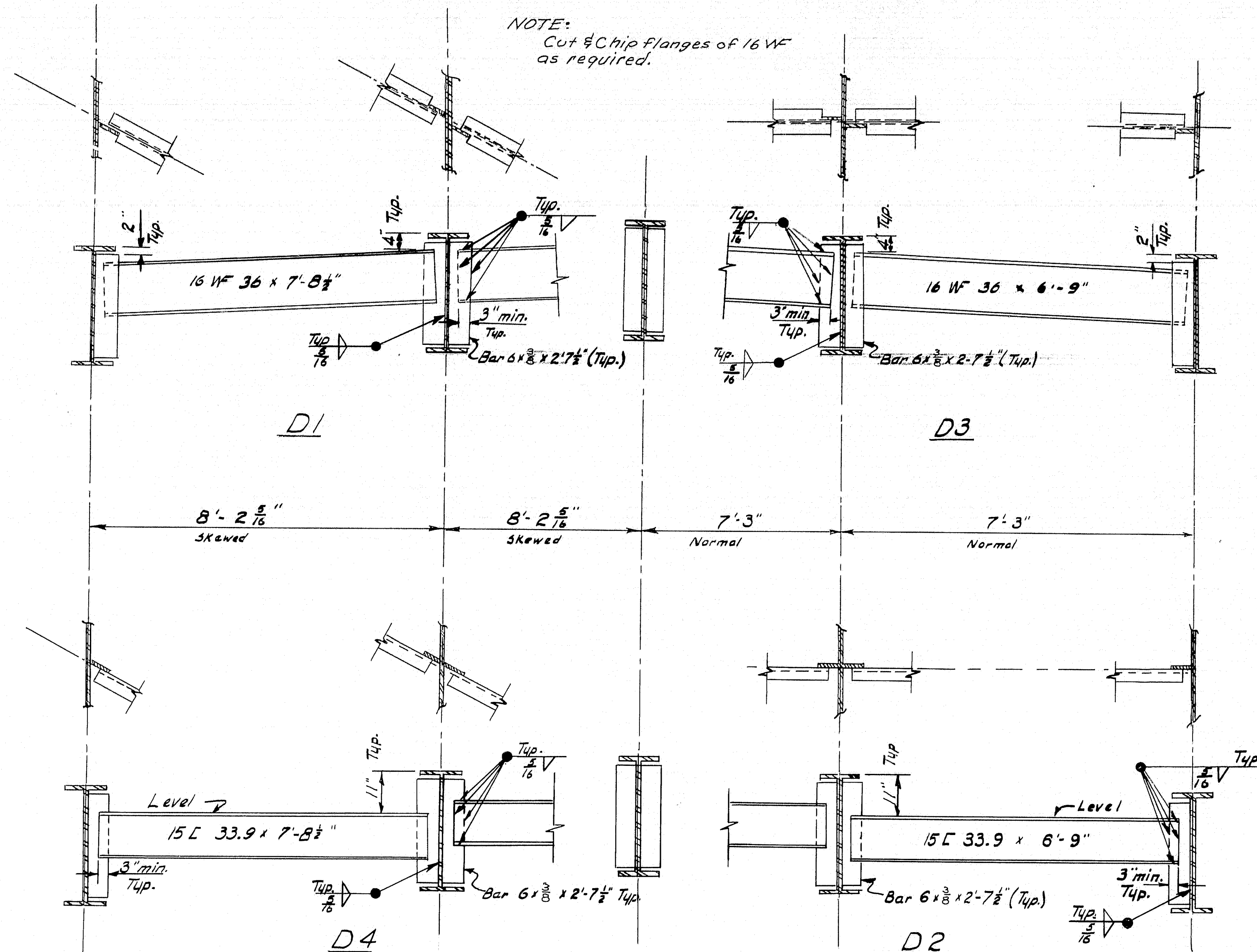


NOTES:

- Stringers and cover plates shall conform to the latest revision of the Specification R.S.T.M. Designation R36. All other structural steel (Bearings, Diaphragms, and Expansion Dams) shall conform to the latest revision of the Specification R.S.T.M. Designation R7 or R36.
- Field connections may be 3/8" Rivets or 3/4" High Tensile Strength Bolts.

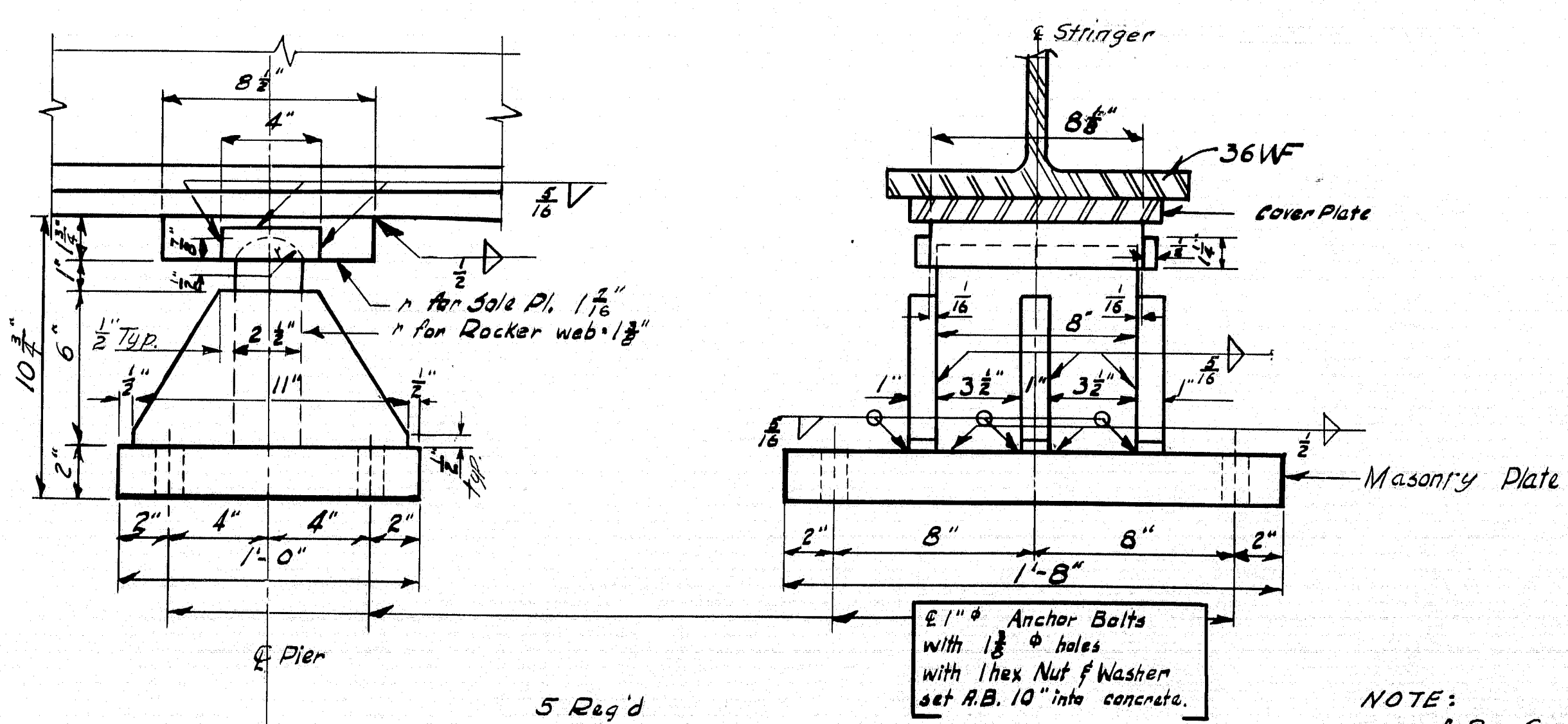
DESIGN - C.D.H.	DETAIL - J.H.W.	BRIDGE NO.
TRACE - P.H.K.	SURVEY -	
CHECK - C.H.B.	PLOT -	
STATE HIGHWAY COMMISSION BRIDGE DIVISION		
ROUTE 69 BRIDGE OVER INTERSTATE HIGHWAY 95 IN THE TOWN OF NEWBURGH		
PENOBSCOT COUNTY STRUCTURAL STEEL - FRAMING PLAN		
SHEET 9 OF 18 AUGUSTA, MAINE MARCH, 1962		



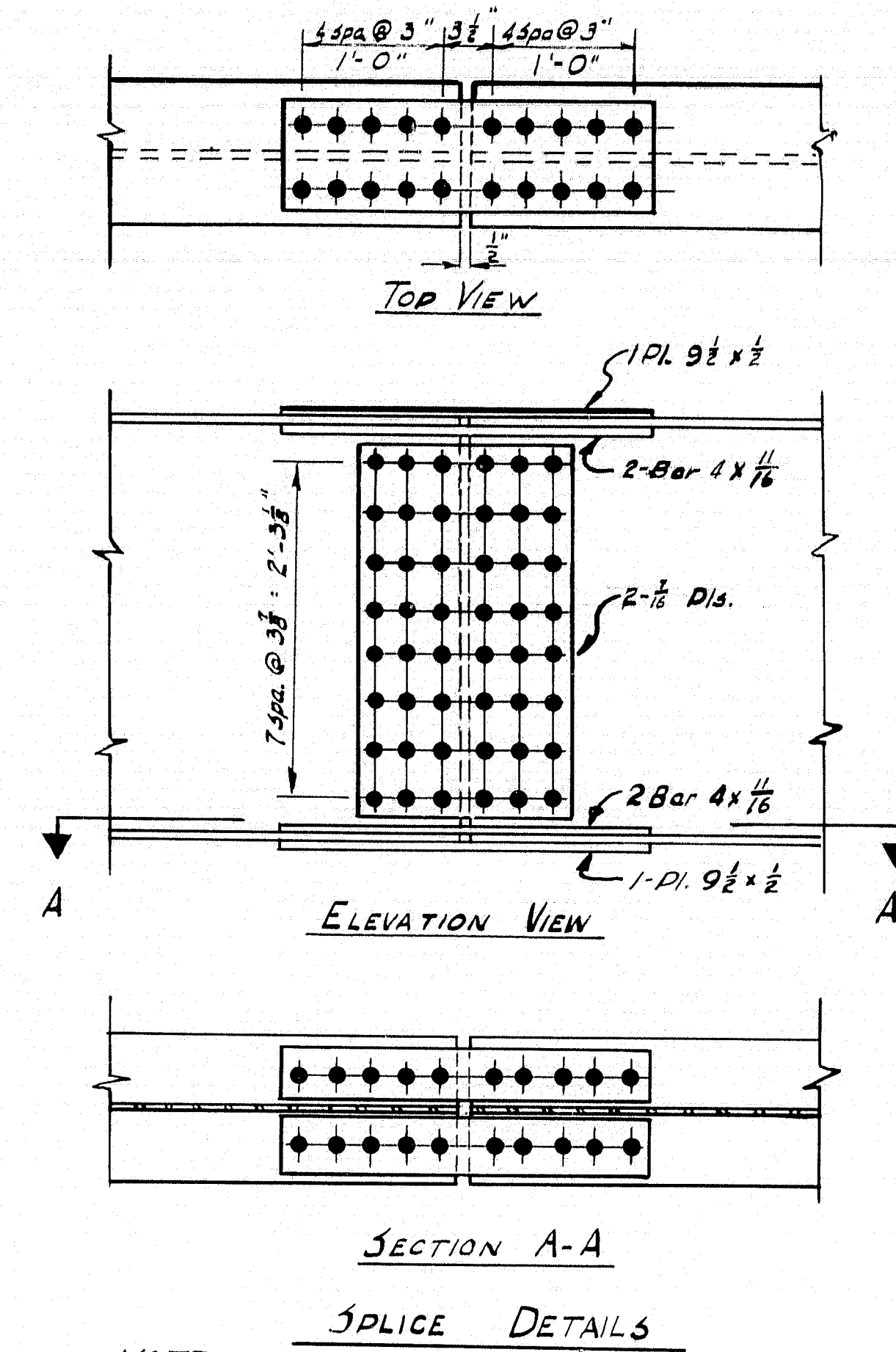


DIAPHRAGMS D1-D2-D3-D4

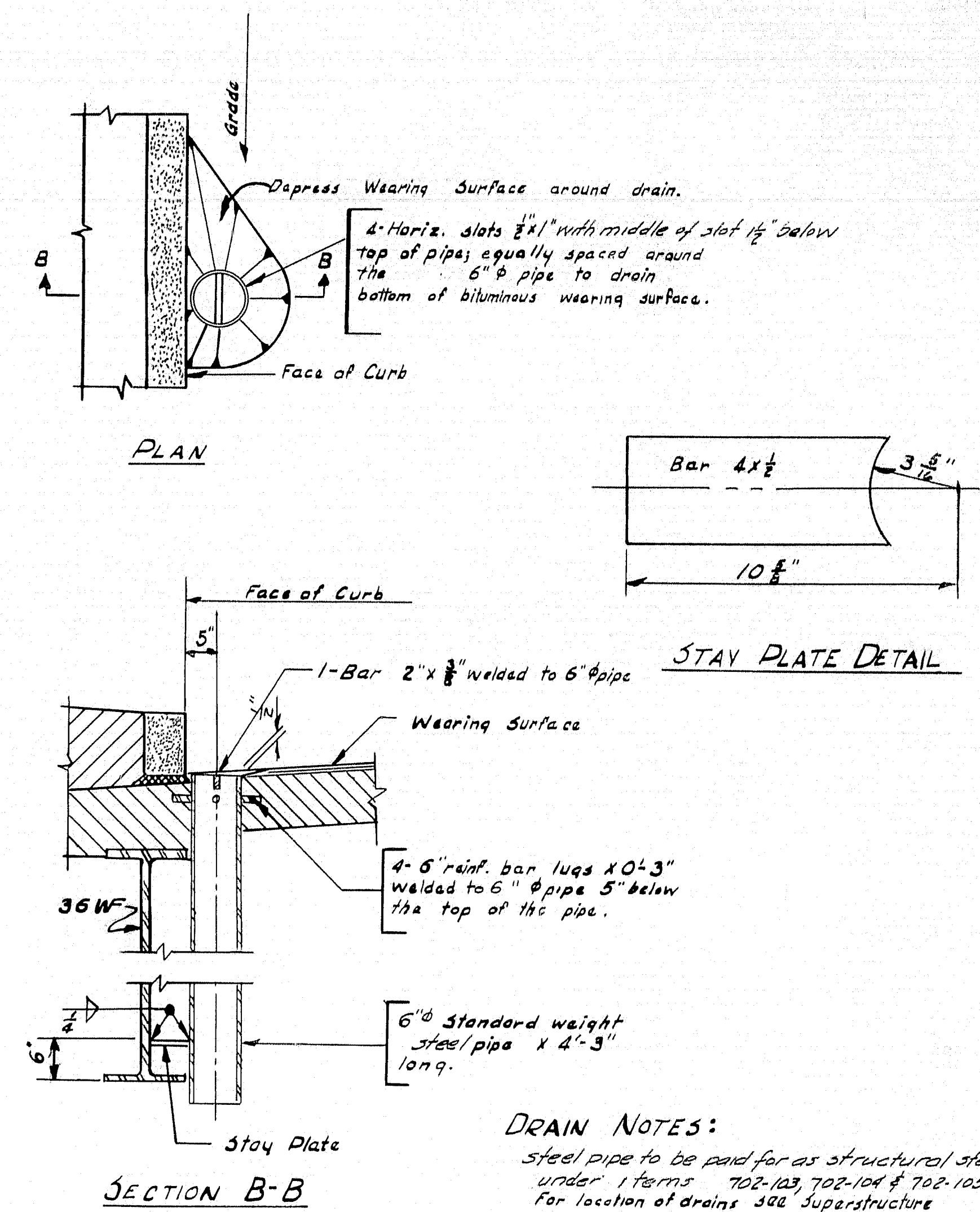
D1- 8 Req'd.
D2- 28 "
D3- 40 "
D4- 20 "



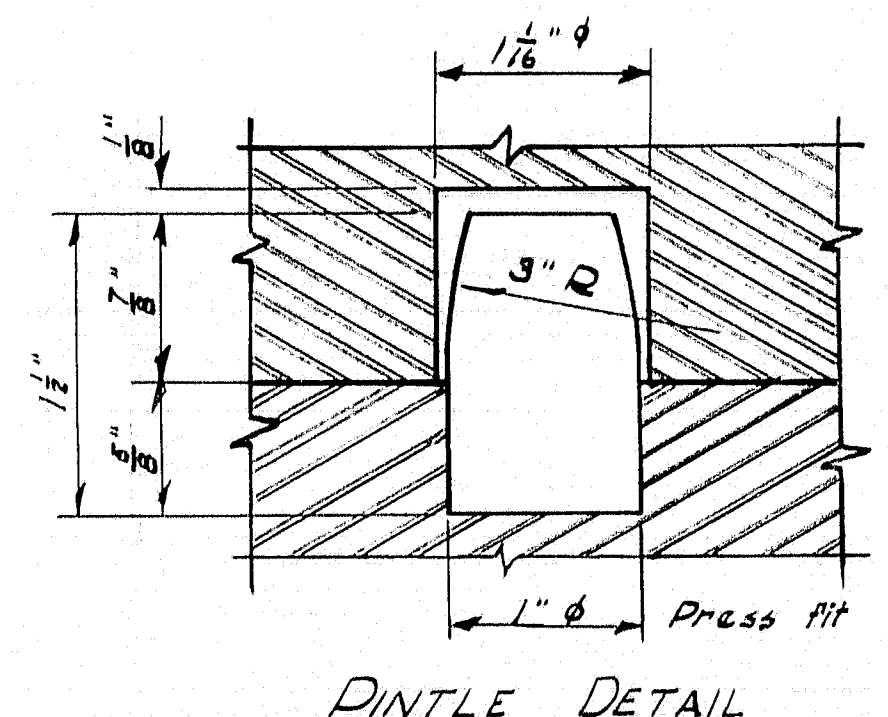
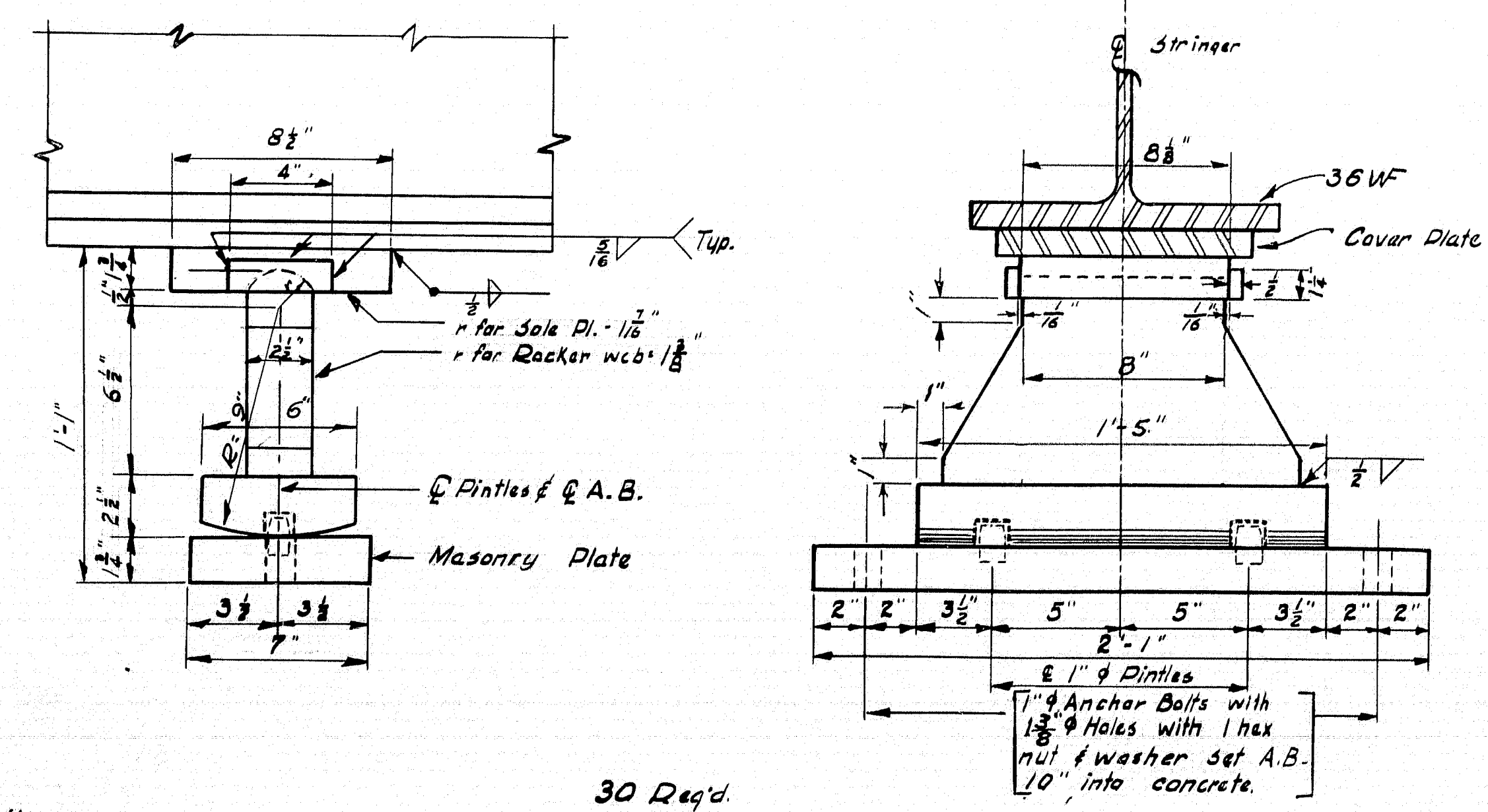
NOTE:
A B = Swaged Anchor Bolts.



NOTE:
Location of, and number of splices indicated on the "Flaming Plan" Sh. #9



DRAIN NOTES:
Steel pipe to be paid for as structural steel under 1 terms 702-103, 702-104 & 702-105. For location of drains see Superstructure Sheets.



DESIGN- C.D.H. DETAIL- J.H.W.
TRACE- J.H.W.
CHECK- J.H.W.

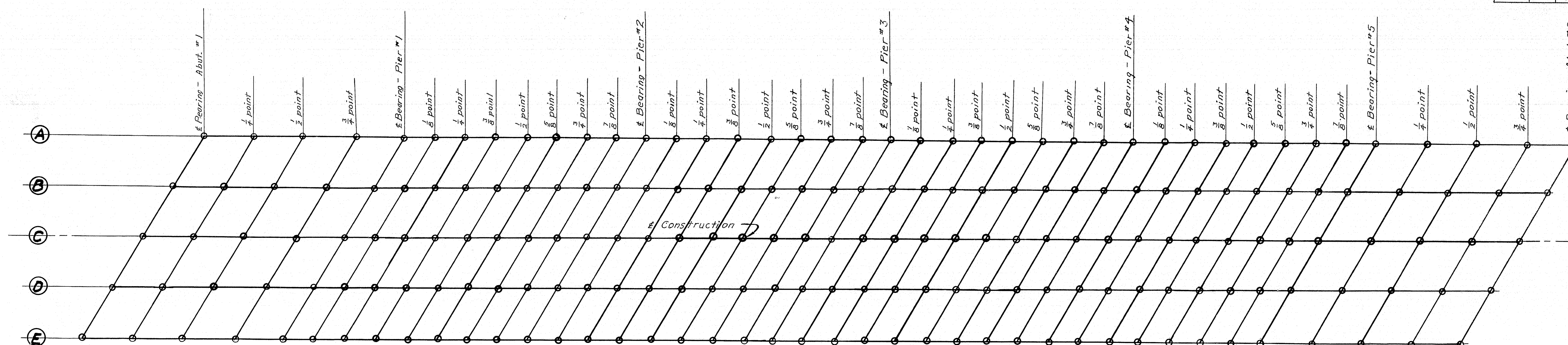
BRIDGE NO. 69
SURVEY- 1962
PLOT- 10/62

STATE HIGHWAY COMMISSION
BRIDGE DIVISION

ROUTE 69 BRIDGE
OVER
INTERSTATE HIGHWAY 95
IN THE TOWN OF
NEWBURGH

PENOBSCOT COUNTY
STRUCTURAL STEEL DETAILS I

SHEET 10 OF 18 AUGUSTA, MAINE, MARCH, 1962

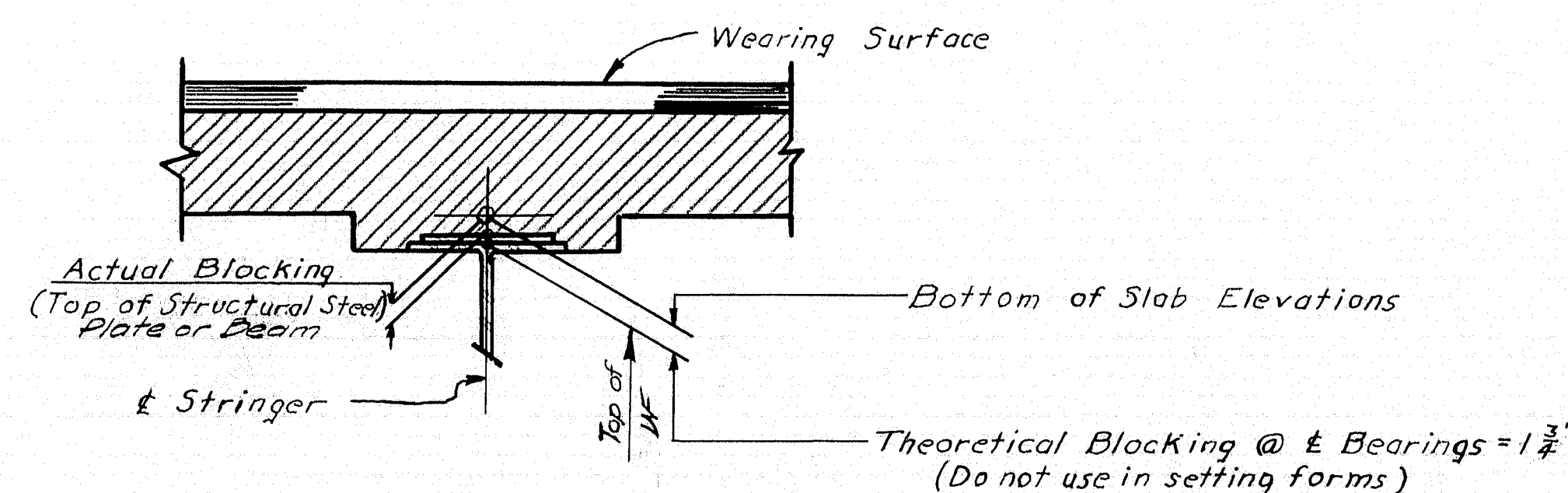


BLOCKING PLAN

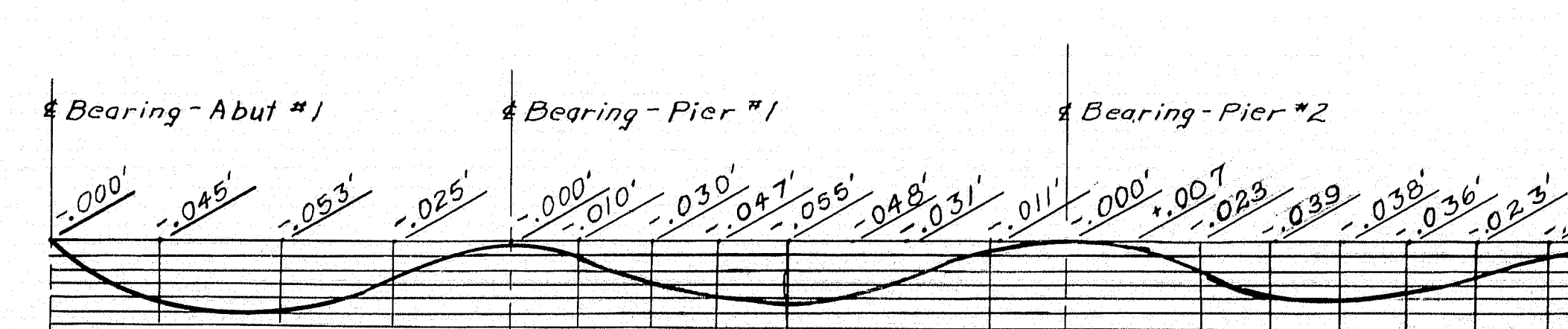
BOTTOM OF SLAB ELEVATIONS																																														
LINE	SPAN 1				SPAN 2								SPAN 3								SPAN 4								SPAN 5								SPAN 6									
	Left Bearing Abut. #1	1/4 point	1/2 point	3/4 point	Left Bearing Pier #1	1/4 point	1/2 point	3/4 point	1/2 point	3/4 point	3/8 point	1/2 point	5/8 point	3/4 point	1/2 point	Left Bearing Pier #2	1/4 point	1/2 point	3/8 point	1/2 point	5/8 point	3/4 point	1/2 point	3/8 point	1/2 point	5/8 point	3/4 point	1/2 point	Left Bearing Pier #3	1/4 point	1/2 point	3/8 point	1/2 point	5/8 point	3/4 point	Left Bearing Pier #4	1/4 point	1/2 point	3/8 point	1/2 point	5/8 point	3/4 point	Left Bearing Pier #5	1/4 point	1/2 point	3/8 point
A	202.58	203.17	203.71	204.22	204.73	205.06	205.40	205.73	206.05	206.34	206.62	206.89	207.16	207.46	207.74	208.02	208.27	208.53	208.74	208.97	209.19	209.42	209.64	209.90	210.09	210.30	210.47	210.65	210.81	211.00	211.19	211.37	211.53	211.67	211.79	211.91	212.03	212.26	212.47	212.62	212.73					
B	202.61	203.19	203.73	204.24	204.75	205.08	205.43	205.76	206.08	206.37	206.66	206.93	207.20	207.49	207.77	208.06	208.31	208.57	208.80	209.02	209.25	209.49	209.72	209.95	210.16	210.38	210.55	210.73	210.89	211.08	211.27	211.46	211.62	211.76	211.89	212.01	212.13	212.36	212.57	212.74	212.86					
C	202.63	203.21	203.75	204.26	204.77	205.10	205.45	205.79	206.11	206.41	206.69	206.96	207.24	207.53	207.82	208.09	208.36	208.62	208.85	209.08	209.31	209.55	209.79	210.02	210.25	210.42	210.62	210.80	210.97	211.16	211.36	211.54	211.71	211.85	211.98	212.10	212.23	212.46	212.69	212.86	212.96					
D	202.35	202.93	203.47	203.98	204.49	204.82	205.17	205.51	205.83	206.13	206.42	206.69	206.97	207.27	207.56	207.85	208.11	208.36	208.60	208.83	209.06	209.30	209.55	209.78	210.00	210.21	210.39	210.58	210.75	210.94	211.14	211.32	211.49	211.64	211.77	211.90	212.03	212.26	212.49	212.66	212.79					
E	202.07	202.65	203.19	203.70	204.21	204.54	204.89	205.23	205.55	205.86	206.15	206.43	206.71	207.01	207.31	207.59	207.85	208.11	208.35	208.58	208.82	209.06	209.31	209.54	209.76	209.98	210.16	210.35	210.52	210.72	210.92	211.10	211.28	211.43	211.56	211.69	211.82	212.06	212.29	212.46	212.58					

BLOCKING NOTES

In order that the Wearing Surface may conform to the profile and cross sections shown on these plans, the accompanying tables of "Bottom of Slab Elevations" are given. These elevations, which are computed to compensate for dead load deflections, must be set before slab forms are started.



BLOCKING DETAIL

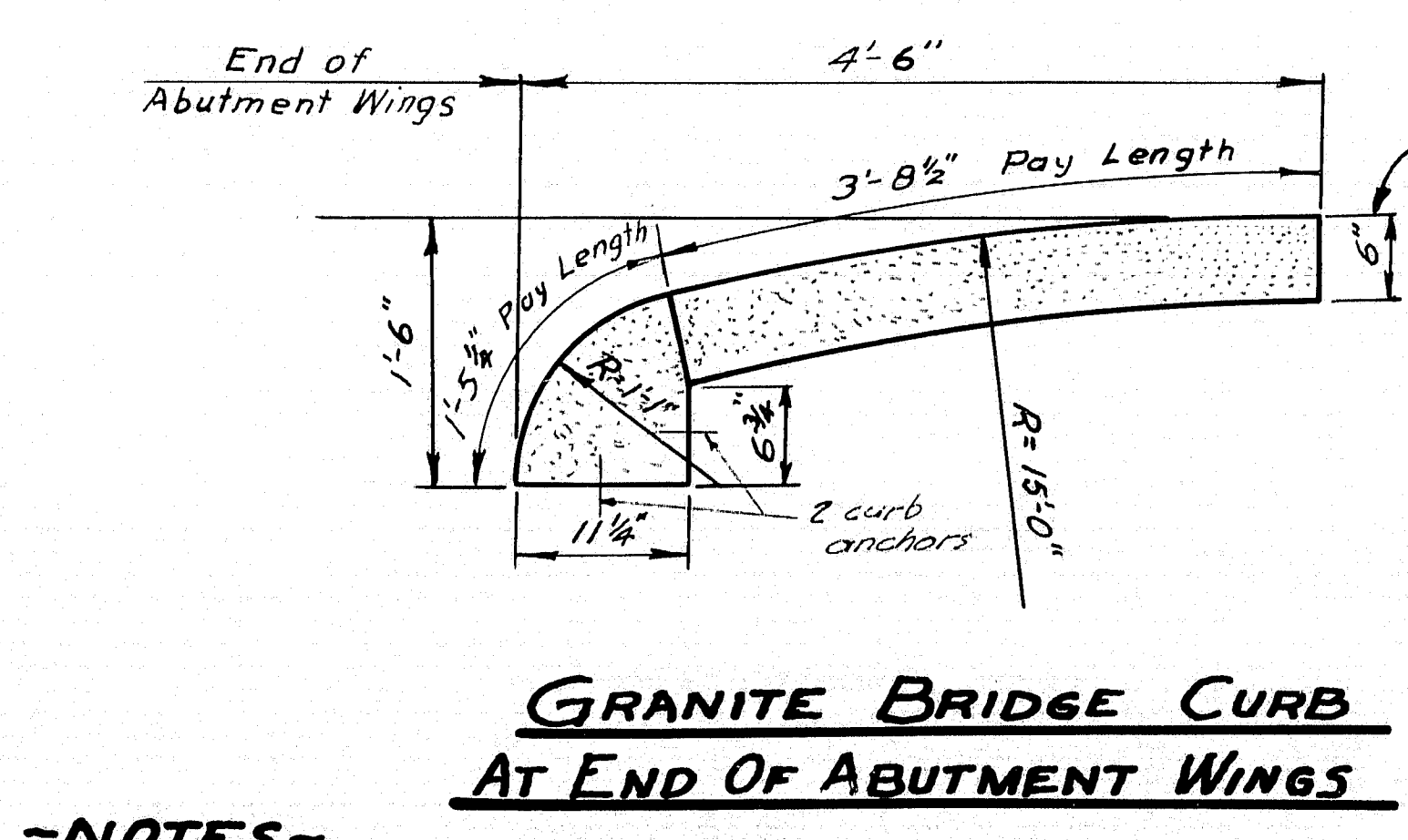
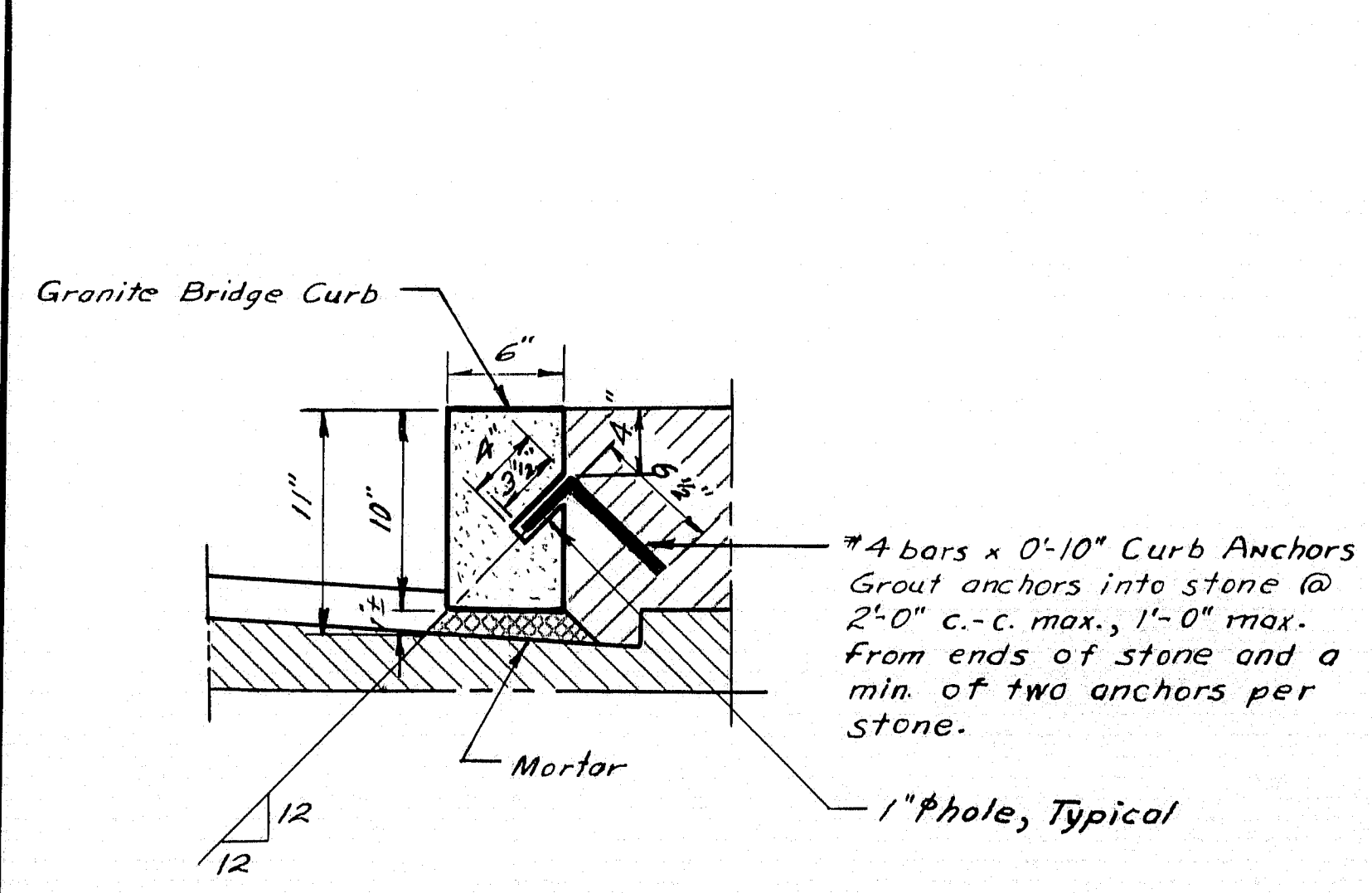
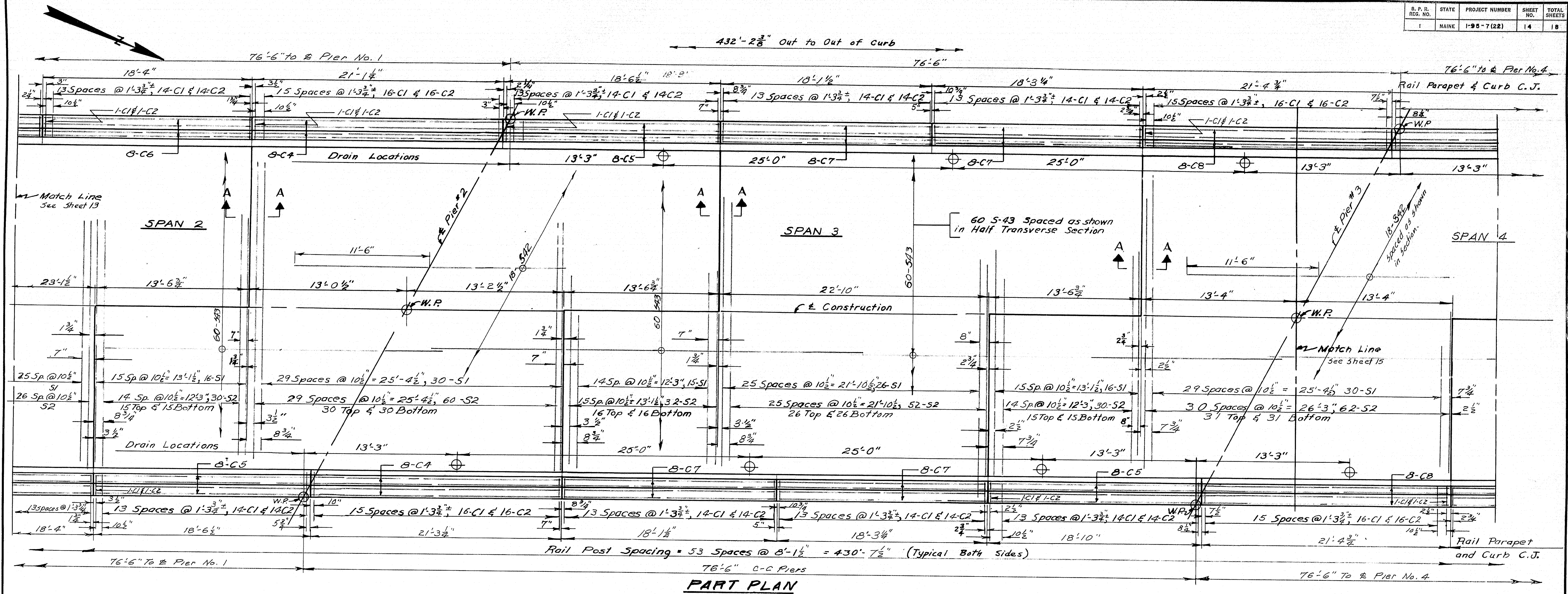


DEFLECTION CURVE

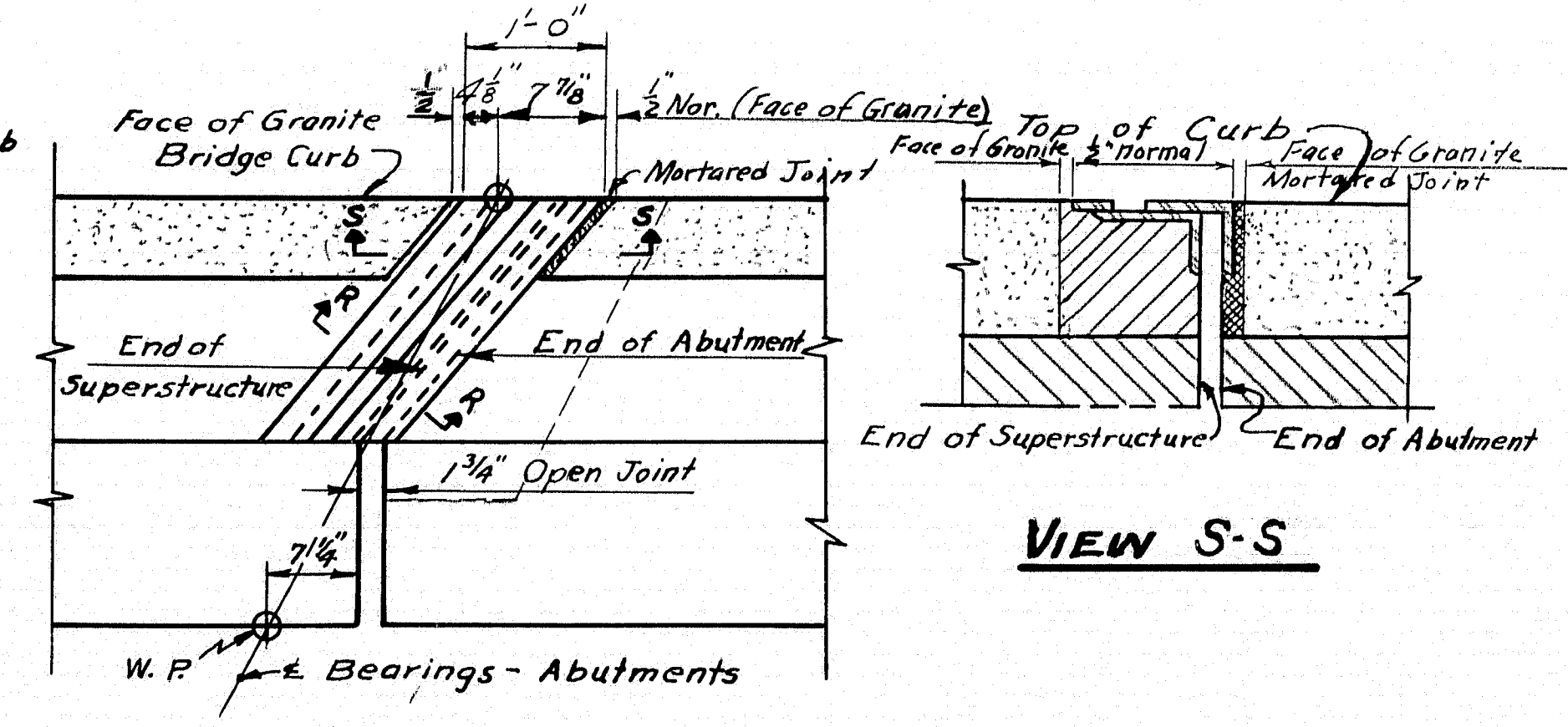
(Deflection for Dead Load minus weight of stringer)

Symm. about Bridge C.
Bearing - Pier #3

DESIGN - CDW
TRACE - J.M.W.
CHECK - J.M.W.
DETAIL - J.M.W.
BRIDGE NO. SURVEY - PLOT -
STATE HIGHWAY COMMISSION
BRIDGE DIVISION
ROUTE 69 BRIDGE
OVER
INTERSTATE HIGHWAY 95
IN THE TOWN OF
NEWBURGH
PENOBSCOT COUNTY
BLOCKING DETAILS
SHEET 12 OF 18 AUGUSTA, MAINE MARCH, 1962



- NOTES-**
1. All Granite Bridge Curb, including that at End of Abutment Wings, shall be paid for under Item 901-21, Granite Bridge Curb.
 2. Joints in Granite Bridge Curb shall be located at the joints between abutment and superstructure & at C.J.s in curbs over piers.
 3. Other joints need not be located at the Rail Parapet and Curb C.J.'s.



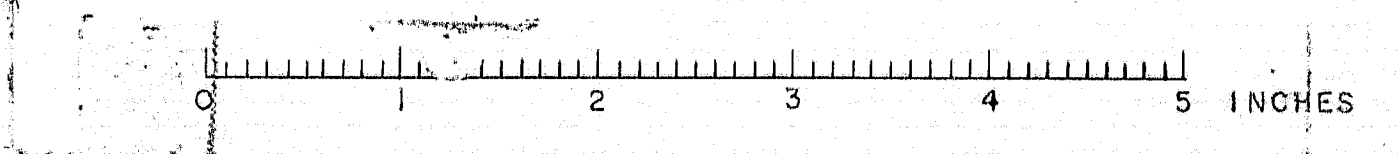
**SKewed JOINT AT END
OF SUPERSTRUCTURE**

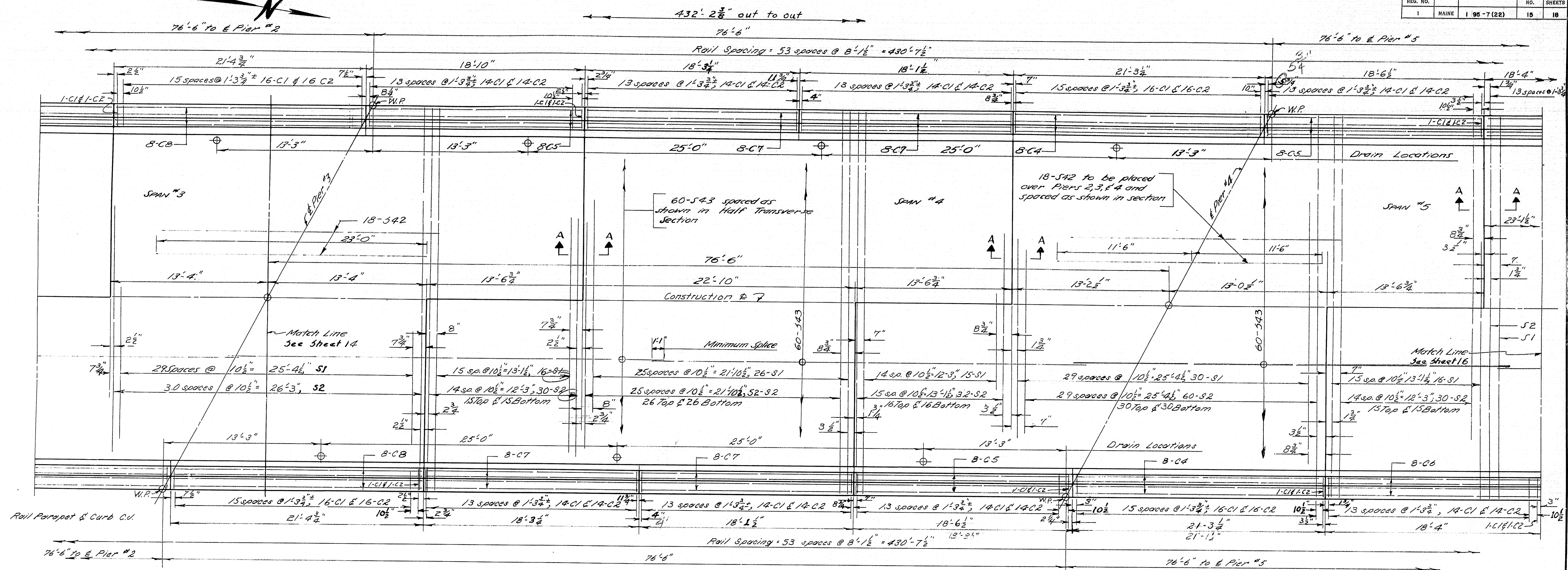
-NOTE-
For Sections, Details, and General Notes see sheets 13, 15, & 16.

DESIGN - CDH	DETAIL - WJM	BRIDGE NO.
TRACE - P.J.M.		SURVEY -
CHECK - Garity		PLOT -

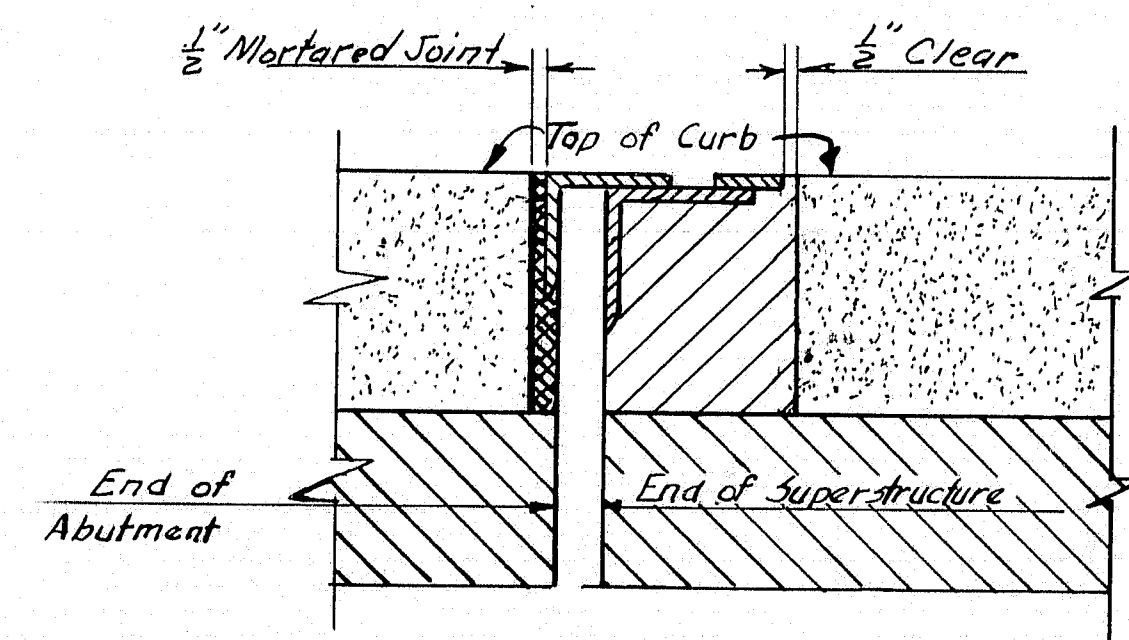
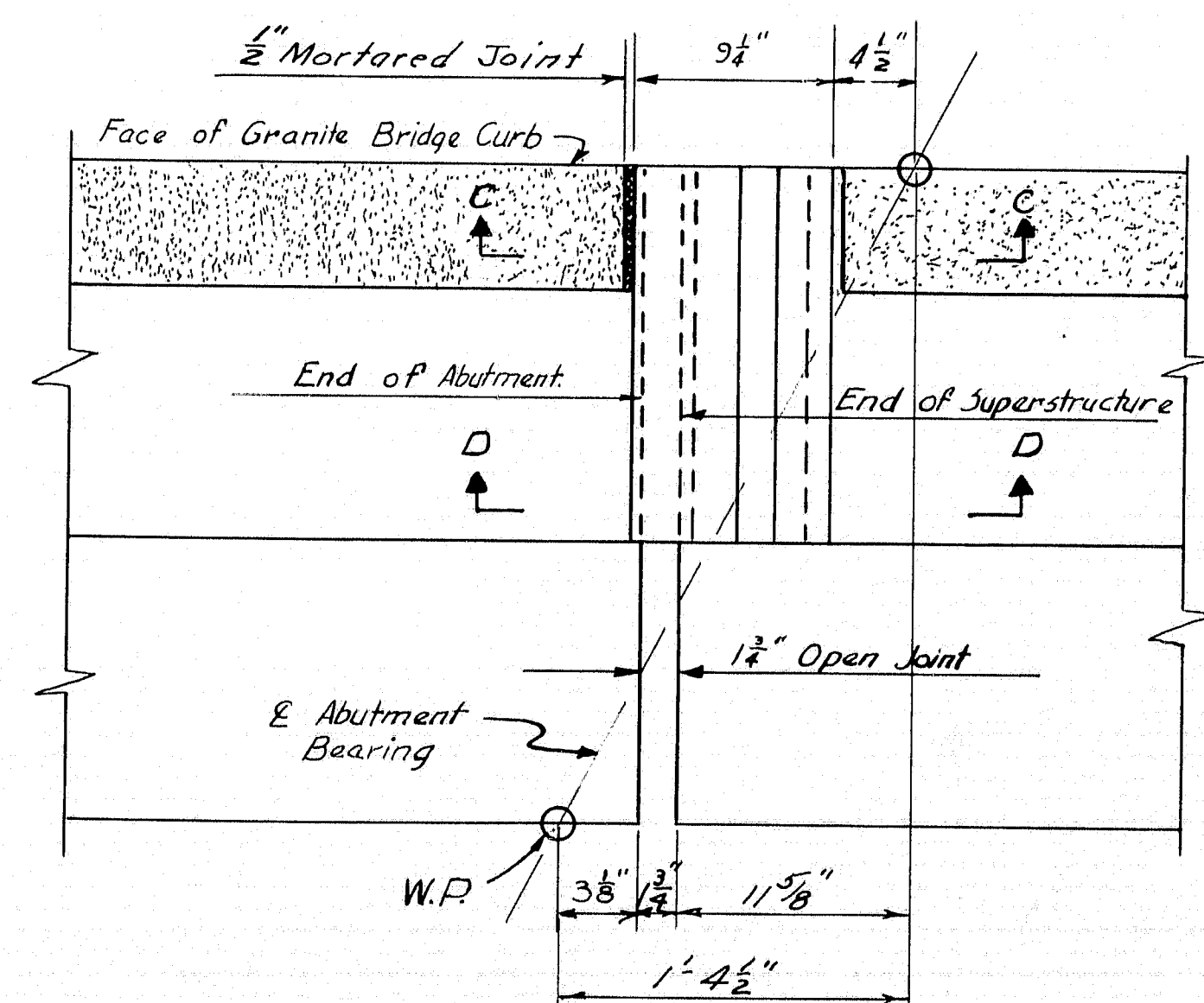
STATE HIGHWAY COMMISSION
BRIDGE DIVISION

ROUTE 69 BRIDGE
OVER
INTERSTATE HIGHWAY 95
IN THE TOWN OF
NEWBURGH
PENOBSCOT COUNTY
SUPERSTRUCTURE - SPANS 2 & 3
SHEET 14 OF 18 AUGUSTA, MAINE, MARCH, 1962

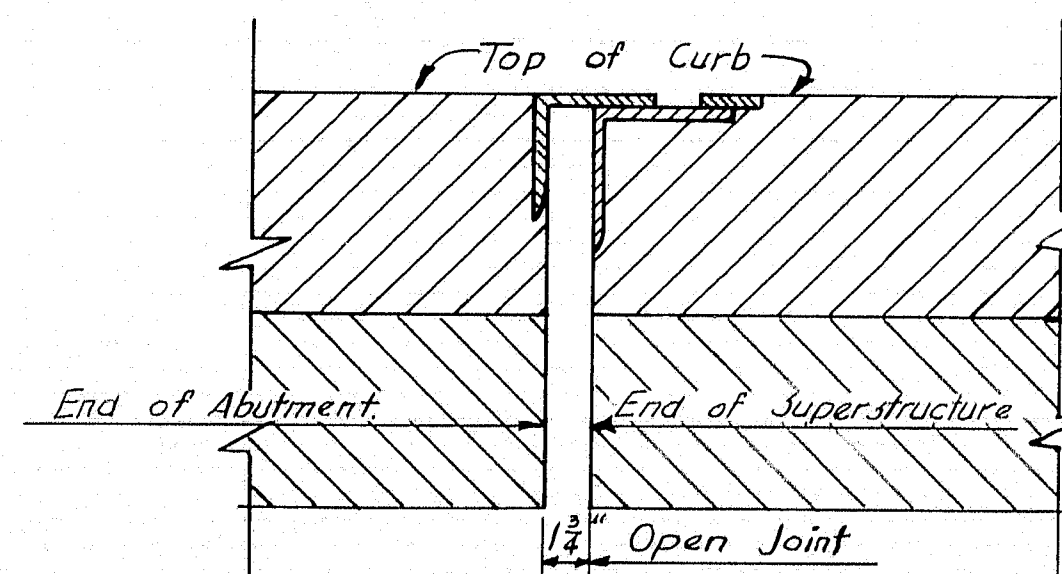




PART PLAN



VIEW C-C



VIEW D-D

NOTE:
For Sections, Details, and General Notes
see Sheets 13, 14, & 16.

NORMAL JOINT AT END OF SUPERSTRUCTURE

DESIGN - C.D.H.	DETAIL - W.J.M.	BRIDGE NO.
TRACE - L.S.P.	SURVEY -	
CHECK - D.C.H.	PLOT -	
STATE HIGHWAY COMMISSION BRIDGE DIVISION		
ROUTE 69 BRIDGE		
OVER		
INTERSTATE HIGHWAY 95		
IN THE TOWN OF		
NEWBURN		
PENOBSCOT COUNTY		
SUPERSTRUCTURE SPANS 485		
SHEET 15 OF 18	AUGUSTA, MAINE	MARCH, 1962

REINFORCING STEEL SCHEDULE

B. P. D. REG. NO. 1 STATE MAINE PROJECT NUMBER 1-95-7(22) SHEET NO. 18 TOTAL SHEETS 18

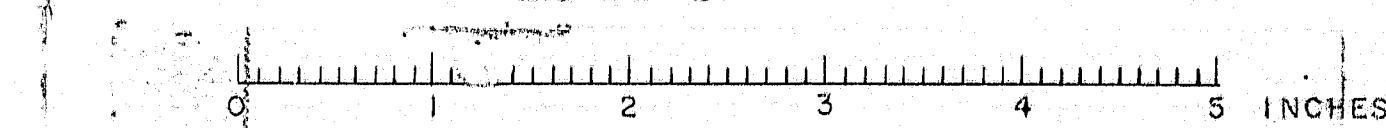
PIERS ✓ <i>Coff</i>					SUPER STRUCTURE ✓ <i>Clarify</i>					ABUTMENTS ✓ <i>Coff</i>				
MARK	SIZE	NO.	LENGTH	LOCATION	MARK	SIZE	NO.	LENGTH	LOCATION	MARK	SIZE	NO.	LENGTH	LOCATION
BENT BARS					BENT BARS					BENT BARS				
P1	#5	20	10'-0"	Cap	S1	#5	476	34'-0"	Slab	A11	#5	20	13'-8"	Wingwalls Abut. #1 Lt. Abut. #2 Rt.
P2	#5	20	10'-7"	-do-	C1	#4	660	5'-3"	Rail parapet	A16	#5	62	2'-2"	Approach Slab to backwall
P3	#5	20	10'-11"	-do-	C2	#4	660	5'-1"	Curb	A19	#5	64	14'-2"	Footings to Abut.
P4	#5	20	11'-5"	-do-						A20	#5	10	8'-11"	-do-
P6	#10	25	35'-8"	-do-						A22	#5	20	13'-2"	Wingwalls Abut. #1 Rt. Abut. #2 Lt.
P9	#9	180	6'-11"	Footings - Column	S3	#5	4	31'-3"	Slab @ Abut.	A26	#5	40	5'-2"	Concrete curb to Wingwall
P15	#4	426	9'-6"	Column	S4	#5	4	30'-8"		A27	#5	10	3'-5"	Concrete curb
P16	#6	192	8'-0"	Footings	S5	#5	4	30'-0"		A28	#5	10	3'-2"	-do-
P17	#8	48	9'-6"	-do- Pier #3	S6	#5	4	29'-3"		A29	#4	52	5'-3"	Sidewalk curb
					S7	#5	4	28'-5"		B21	#5	2	3'-3"	Bridge Seat
					S8	#5	4	27'-7"						
STRAIGHT BARS					STRAIGHT BARS					STRAIGHT BARS				
P5	#10	25	35'-8"	Cap	S9	#5	4	26'-9"		A1	#6	54	5'-9"	Footings
P7	#8	40	9'-6"	-do-	S10	#5	4	26'-0"		A2	#5	2	7'-11"	-do-
P8	#6	20	35'-8"	-do-	S11	#5	4	25'-1"		A3	#5	2	8'-5"	-do-
P10	#9	36	30'-3"	Column Pier 1	S12	#5	4	24'-3"		A4	#5	4	8'-11"	-do-
P11	#9	36	32'-9"	Column Pier 2	S13	#5	4	23'-4"		A5	#5	4	9'-6"	-do-
P12	#9	36	31'-11"	Column Pier 3	S14	#5	4	22'-7"		A6	#5	4	10'-0"	-do-
P13	#9	36	29'-7"	Column Pier 4	S15	#5	4	21'-9"		A7	#5	4	10'-7"	-do-
P14	#9	36	27'-11"	Column Pier 5	S16	#5	4	21'-0"		A8	#5	2	11'-0"	-do-
					S17	#5	4	20'-1"		A9	#6	2	11'-7"	-do-
					S18	#5	4	19'-4"						
					S19	#5	4	18'-6"		A12	#5	8	8'-0"	Wingwall Abut. #1 Lt. Abut. #2 Rt.
SUPERSTRUCTURE STRAIGHT BARS					S20	#5	4	17'-9"		A14	#7	22	5'-6"	Footings
S29	#5	4	10'-3"	Slab @ Abutments	S21	#5	4	16'-11"		A15	#7	12	39'-3"	-do-
S30	#5	4	9'-4"		S22	#5	4	16'-0"		A17	#6	24	16'-7"	Approach slab
S31	#5	4	8'-7"		S23	#5	4	15'-2"		A18	#4	40	30'-0"	-do-
S32	#5	4	7'-8"		S24	#5	4	14'-4"		A21	#5	134	6'-2"	Backwall
S33	#5	4	6'-11"		S25	#5	4	13'-6"		A23	#5	40	8'-0"	Wingwalls
S34	#5	4	6'-1"		S26	#5	4	12'-8"		A24	#5	8	7'-11"	Wingwalls Abut. #1 Rt. Abut. #2 Lt.
S35	#5	4	5'-3"		S27	#5	4	11'-10"		A25	#5	56	3'-1"	Footings Wingwalls
S36	#5	4	4'-5"		S28	#5	4	11'-0"	Slab @ Abut.					
S37	#5	4	3'-7"		S2	#5	950	32'-11"	Slab					
S38	#5	4	2'-8"		S41	#5	36	21'-0"	Slab @ Piers #1 f3					
S39	#5	4	2'-3"	Slab @ Abutments	S42	#5	54	23'-0"	Slab @ Piers #2,3 f4					
C7	#4	64	17'-10"	Rail Parapet & Curbs	S43	#5	720	37'-0"	Slab					
C8	#4	16	21'-0"	" " "	C3	#4	32	20'-6"	Rail parapet & curbs					
C9	#4	32	22'-0"	" " "	C4	#4	64	20'-9"						
					C5	#4	80	18'-4"						
					C6	#4	64	18'-0"	Rail parapet & curbs					

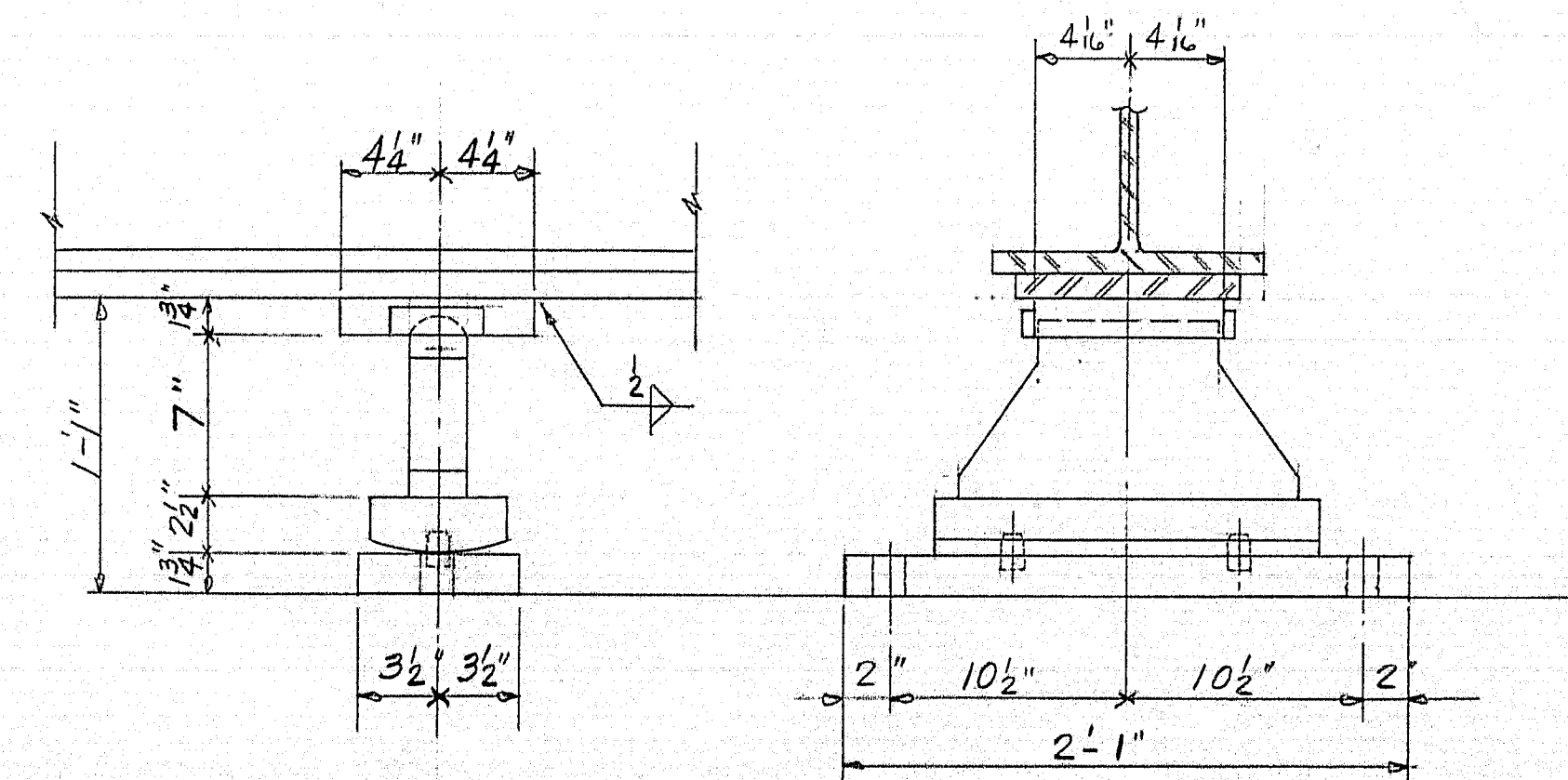
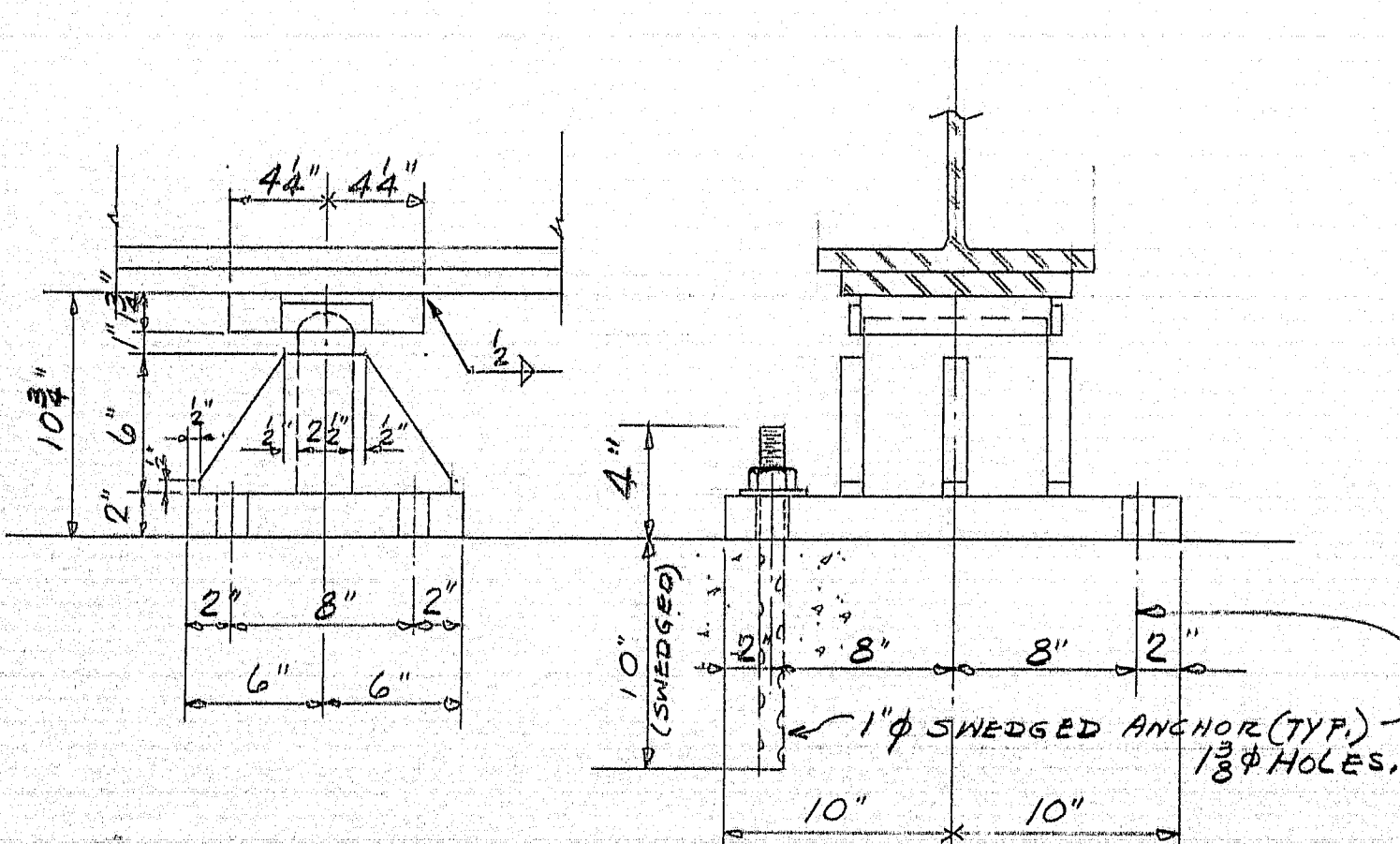
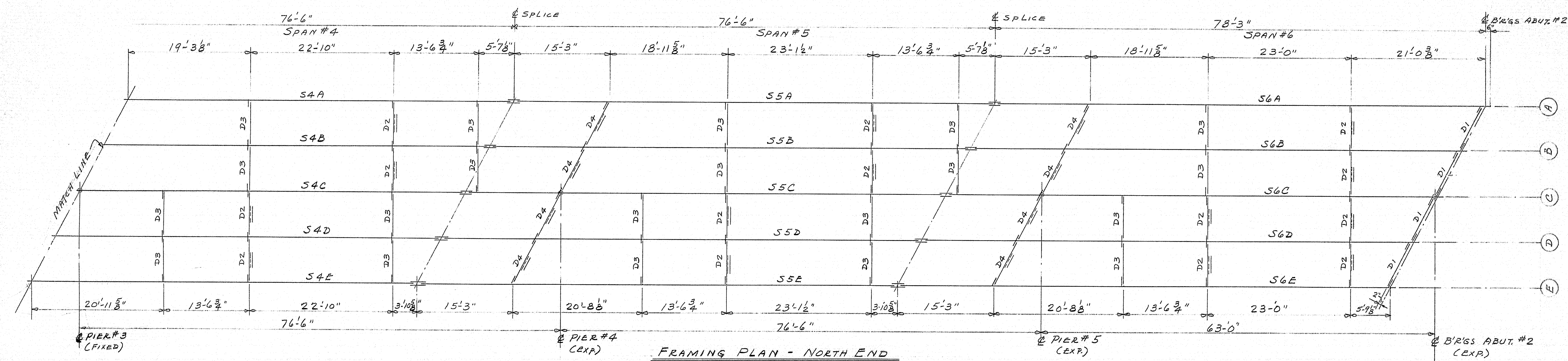
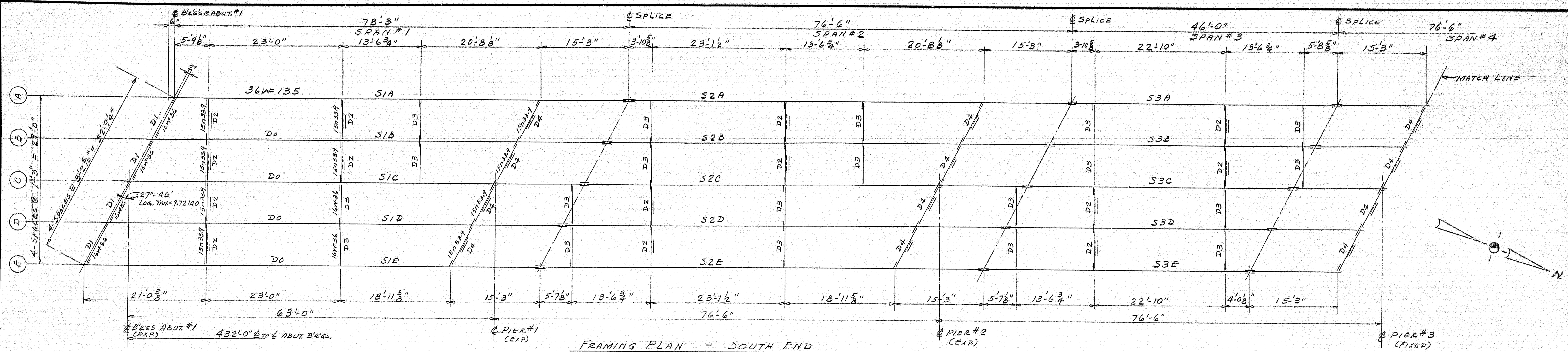
NOTES:

- All dimensions to ϕ bars.
- All reinforcing steel to be intermediate grade steel, $f_y = 20,000$ psi.

DESIGN - G.D.H. DETAIL - J.H.W. BRIDGE NO. SURVEY - PLOT -
 CHECK - R. M. W.
 STATE HIGHWAY COMMISSION
 BRIDGE DIVISION
ROUTE 69 BRIDGE
 OVER
INTERSTATE HIGHWAY 95
 IN THE TOWN OF
NEWBURGH
PENOBSCOT COUNTY
 REINFORCING STEEL SCHEDULE
 SHEET 18 OF 18 AUGUSTA, MAINE MARCH, 1962

85-176

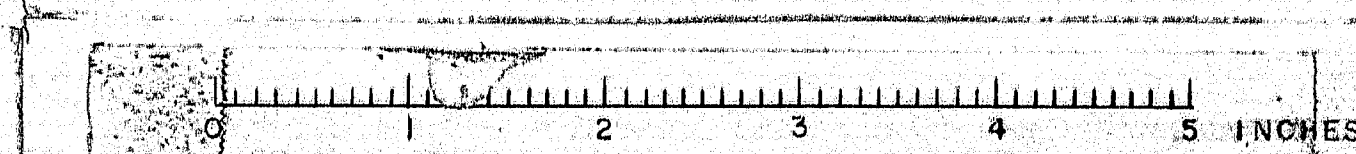




SEE DWG. E2 FOR DETAILS OF SPLICE
AND TYP. DIAPHRAM CONNECTIONS.
SHOP CONNECTIONS: WELD
FIELD CONNECTIONS: 3/8" H.S. BOLTS.
HOLES: 1/8" UNLESS NOTED.
PAINT: STATE SPECS.

APP. EXCEPT AS NOTED 9-20-62

FRAMING PLAN			Bancroft & Martin Rolling Mills Company	
PRINT ISSUE			Brewer, Maine	
1	ERECT	11-30-62	ROUTE 69 BRIDGE	
3	DIST.	10-9-62	NEWBURGH MAINE	
1	SHOP	10-9-62	CUSTOMER CALLAHAN BROS.	
3	F.A.	9-19-62	DESIGNER STATE HIGHWAY DEPT.	
3	FIA	9-4-62	ORDER VERBAL	
DRAWN	8-25-62	E.M.	DWG. B62-168-F1	
REVISION				
REVISION				
REVISION				



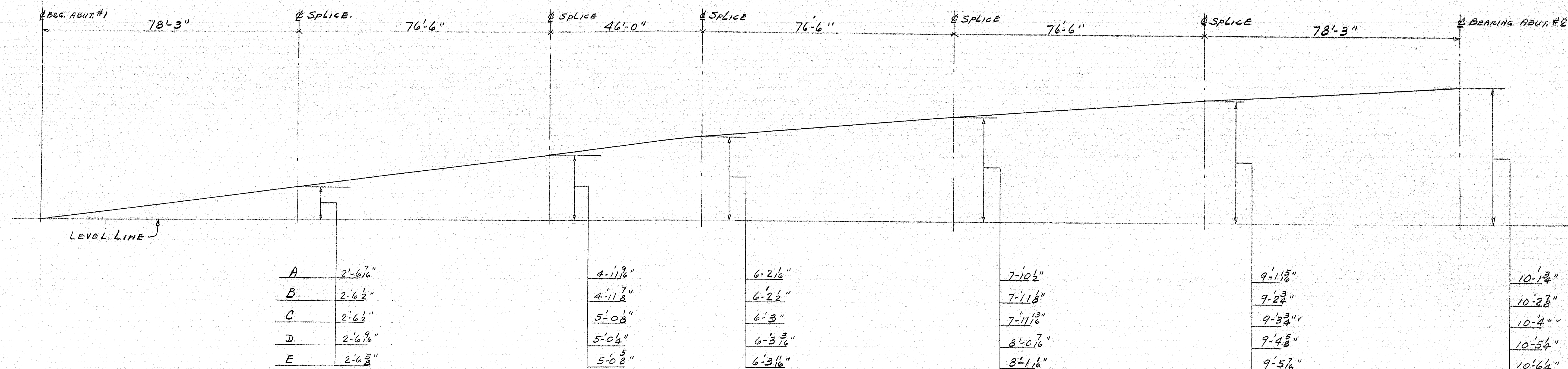
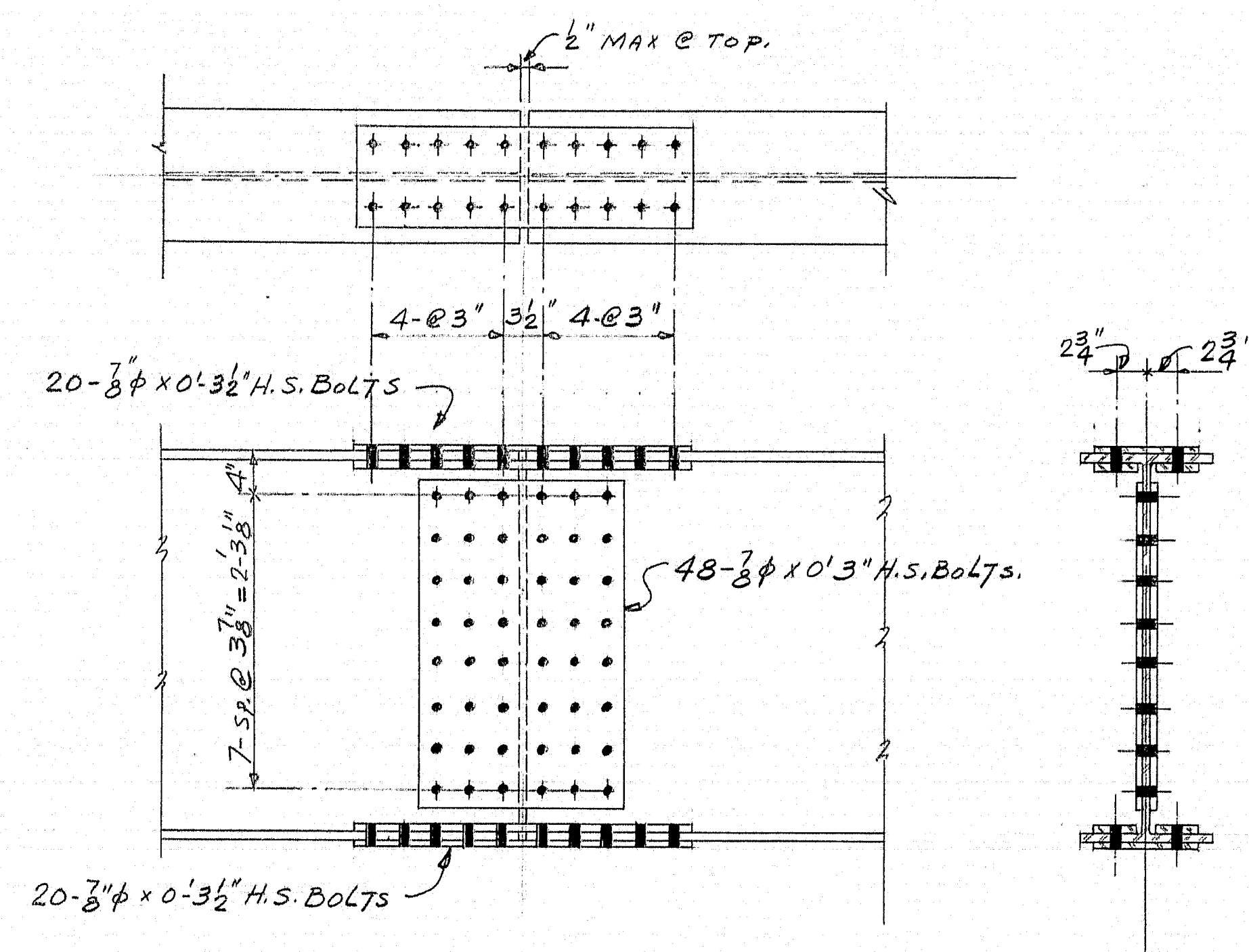
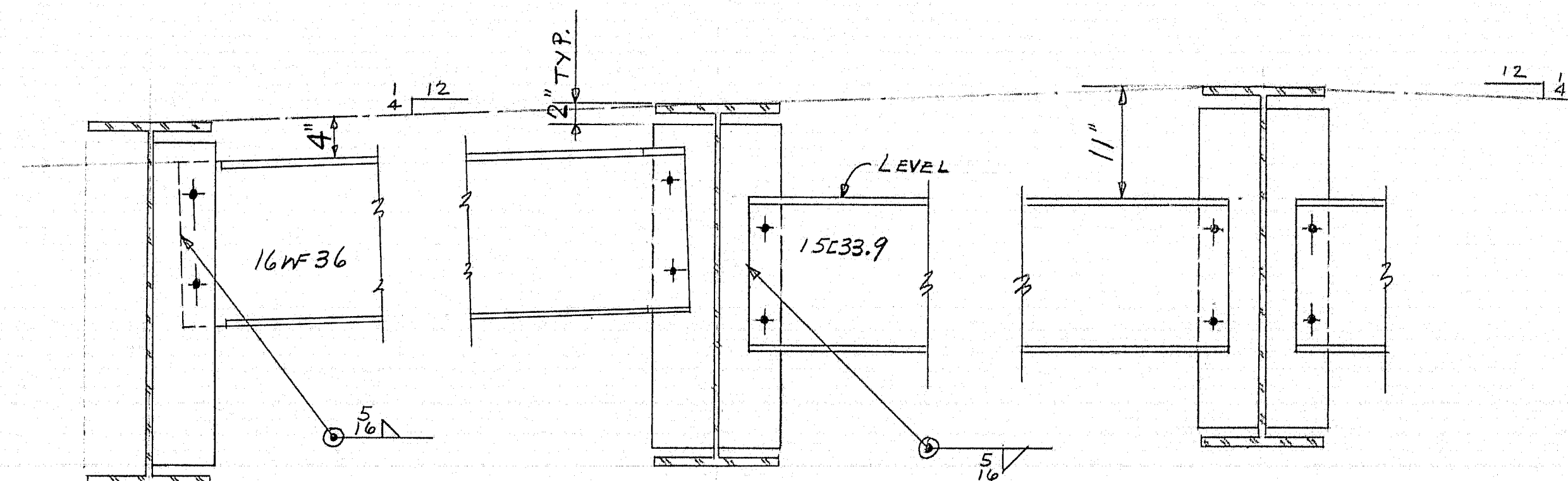


DIAGRAM OF STRINGER ELEVATION
COMPUTATION OF DIMENSION IS BASED ON "BEARING AREA ELEVATIONS".



TYPICAL SPLICE DETAIL



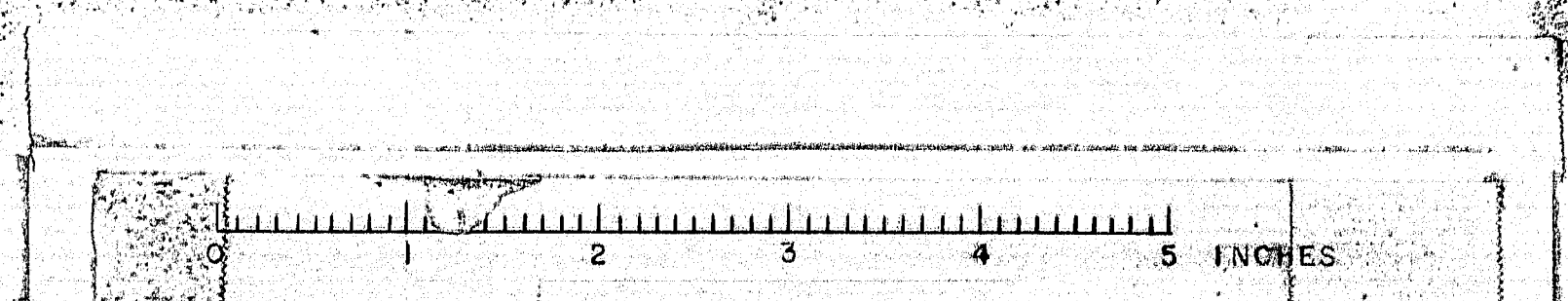
DIAPHRAM DETAILS

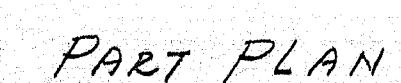
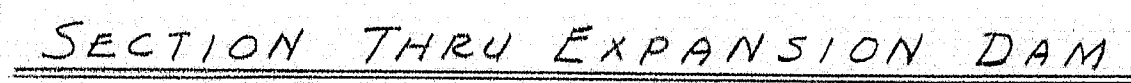
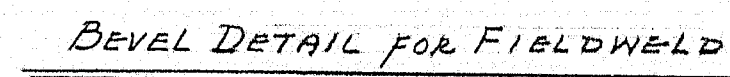
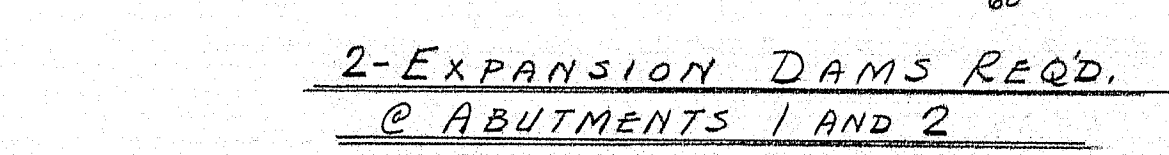
SHOP CONNECTIONS: WELD
FIELD CONNECTIONS: 3/8" H.S. BOLTS
HOLES: 1/8" UNLESS NOTED
PAINT: STATE SPECS.

APP. AS NOTED 9-28-62

DIAGRAM OF STRINGER ELEVATIONS & CONST. DETAILS.

PRINT ISSUE		Bancroft & Martin Rolling Mills Company Brewer, Maine
REVISION	DATE	
1	ERECT 11-30-62	ROUTE 69 BRIDGE NEWBURGH MAINE
3	ERECT 10-9-62	
1	SHOP 10-9-62	CUSTOMER CALLAHAN BROS. DESIGNER STATE HIGHWAY DEPT.
2	F.A. 9-24-62	
DRAWN	9-17-62 E.M.	ORDER VERBAL
REVISION		
REVISION		DWG. B62-168-E2
REVISION		

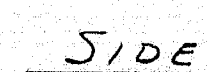
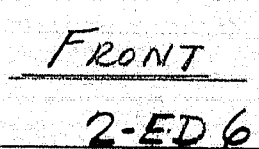
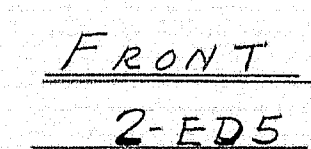
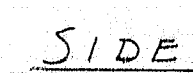
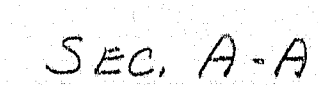




STRUCTURAL STEEL IS ITEM 702-103

APP. EXCEPT AS NOTED 9-20-62

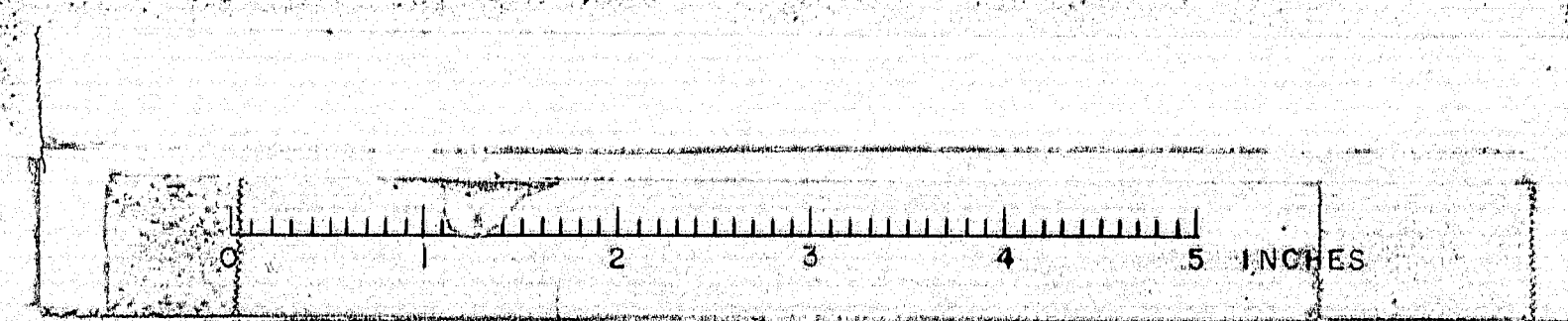
EXPANSION DAMS				
PRINT ISSUE		<i>Bancroft & Martin Pottery Mills Company</i> <i>Brewer, Maine</i> ROUTE #69 BRIDGE NEWBURGH MAINE		
3	ERECT	9-27-62	CUSTOMER <u>CALLAHAN BROS.</u> DESIGNER <u>STATE HIGHWAY DEPT</u>	
5	SHOP	9-27-62		
3	F.A.	9-19-62		
3	F.A.	9-4-62		
DRAWN		9-1-62	E.M.	ORDER <u>VERBAL</u> DWG. <u>B62-168-52</u>
REVISION				
REVISION				
REVISION				

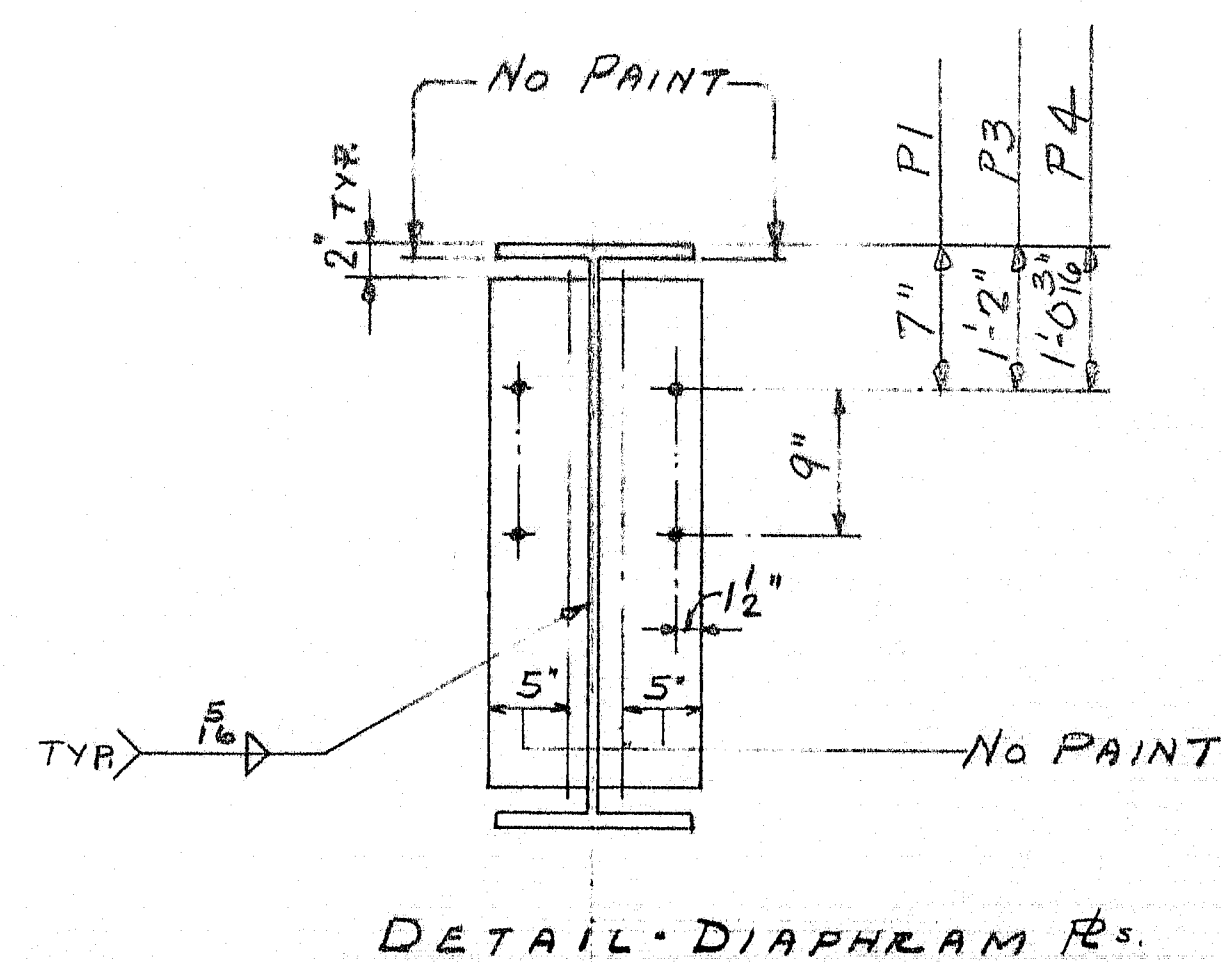
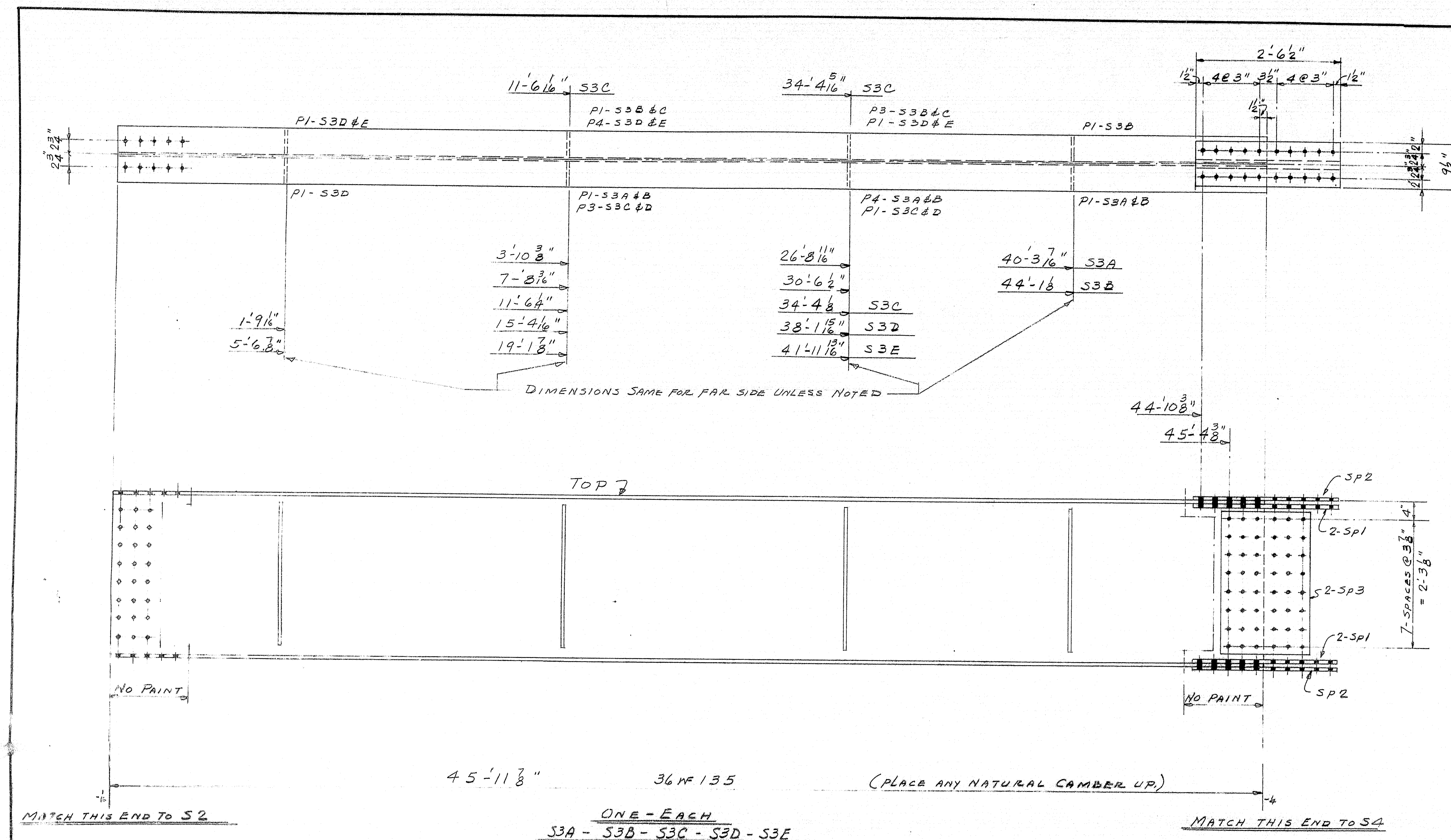


SHOP CONNECTIONS: WELD
FIELD CONNECTIONS: —
HOLES: AS NOTED
PAINT: STATE SPECS

APP. EXCEPT AS NOTED 9-20-62

SIDWALK EXPANSION DAMS					
PRINT ISSUE					
3	ELECT.	9-27-62	<i>Bancroft & Martin Rollins Mills Company Brewer, Maine</i> ROUTE 69 BRIDGE NEWBURGH MAINE CUSTOMER <u>CALLAHAN BROS.</u> DESIGNER <u>STATE HIGHWAY DEPT</u> ORDER <u>VERBAL</u> DWG. <u>B62-168-S3</u>		
5	SHP.	9-27-62			
3	F.A.	9-19-62			
3	F.A.	9-4-62			
DRAWN	9-3-62	E.M.			
REVISION					
REVISION					
REVISION					





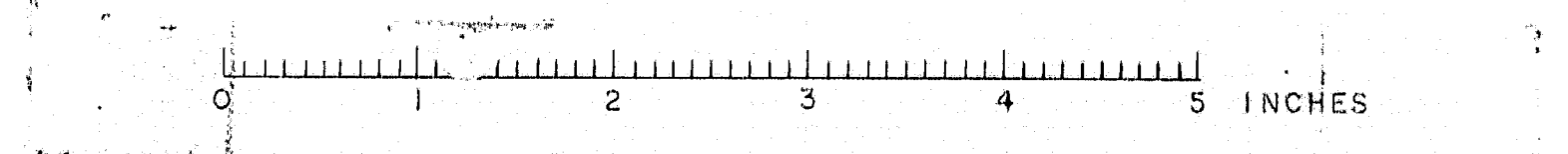
SHIP		BILL OF MATERIAL					DWG. D62-168-S	
MARK	NO.	MARK	SHAPE	LENGTH	WT.	DESCRIPT	REMARKS	
S3A	1		36WF135	45 11 3/8		12 #	A3	
S3B	1		DO	45 11 3/8		12		
S3C	1		DO	45 11 3/8		12		
S3D	1		DO	45 11 3/8		12		
S3E	1		DO	45 11 3/8		2		
	20	SP1	R4x1 1/2	2 6 1/2		2	A7	
	10	SP2	R9 1/2x2	2 6 1/2		2		
	10	SP3	R18 1/2x1 1/2	2 6 1/2		2		
	14	P1	BAR 6x3	2 7 1/2		2 #		
	4	P3	DO	2 7 1/2		2		
	4	P4	DO	2 7 1/2		2		
	1		5" WELD	11 1/2				

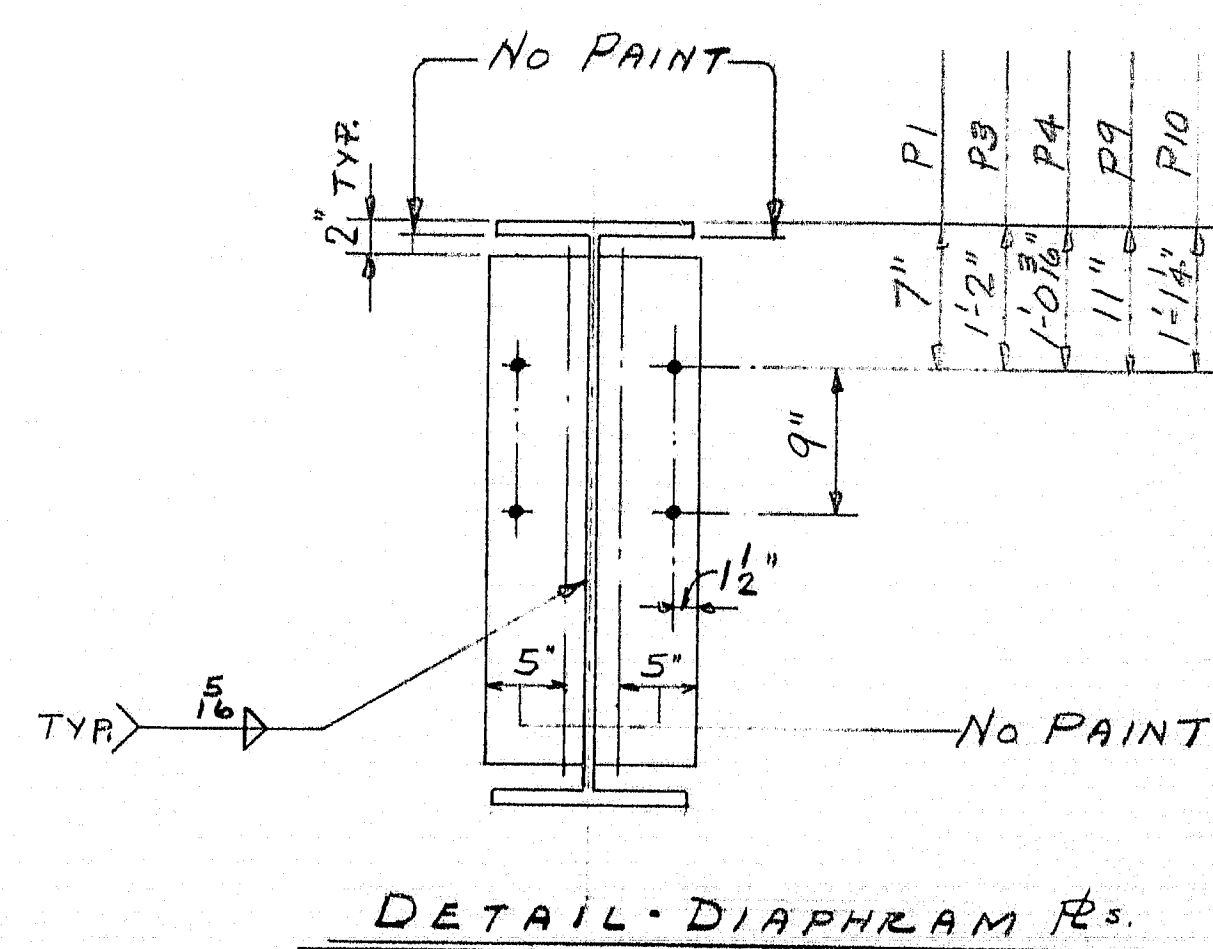
SHOP CONNECTIONS: WELD
 FIELD CONNECTIONS: 8" H.S. BOLTS
 HOLES: 1 1/8" SEE NOTE ON DWG 64
 PAINT: STATE SPECS.

APP. AS NOTED 9-28-62

S3 - STRINGERS			Bancroft & Martin Rolling Mills Company	
PRINT ISSUE			Brewer, Maine	
3	ERECT	10-9-62	ROUTE 69 BRIDGE	
5	SHOP	10-9-62	NEWBURGH MAINE	
2	F.A.	9-24-62	CUSTOMER CALLAHAN BROS.	
DRAWN	9-22-62	E.M.	DESIGNER STATE HIGHWAY DEPT.	
REVISION	10-9-62	E.M.		
REVISION			ORDER VERBAL	
REVISION			DWG. B62-168-S6	

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SHOP CONNECTIONS: WELD

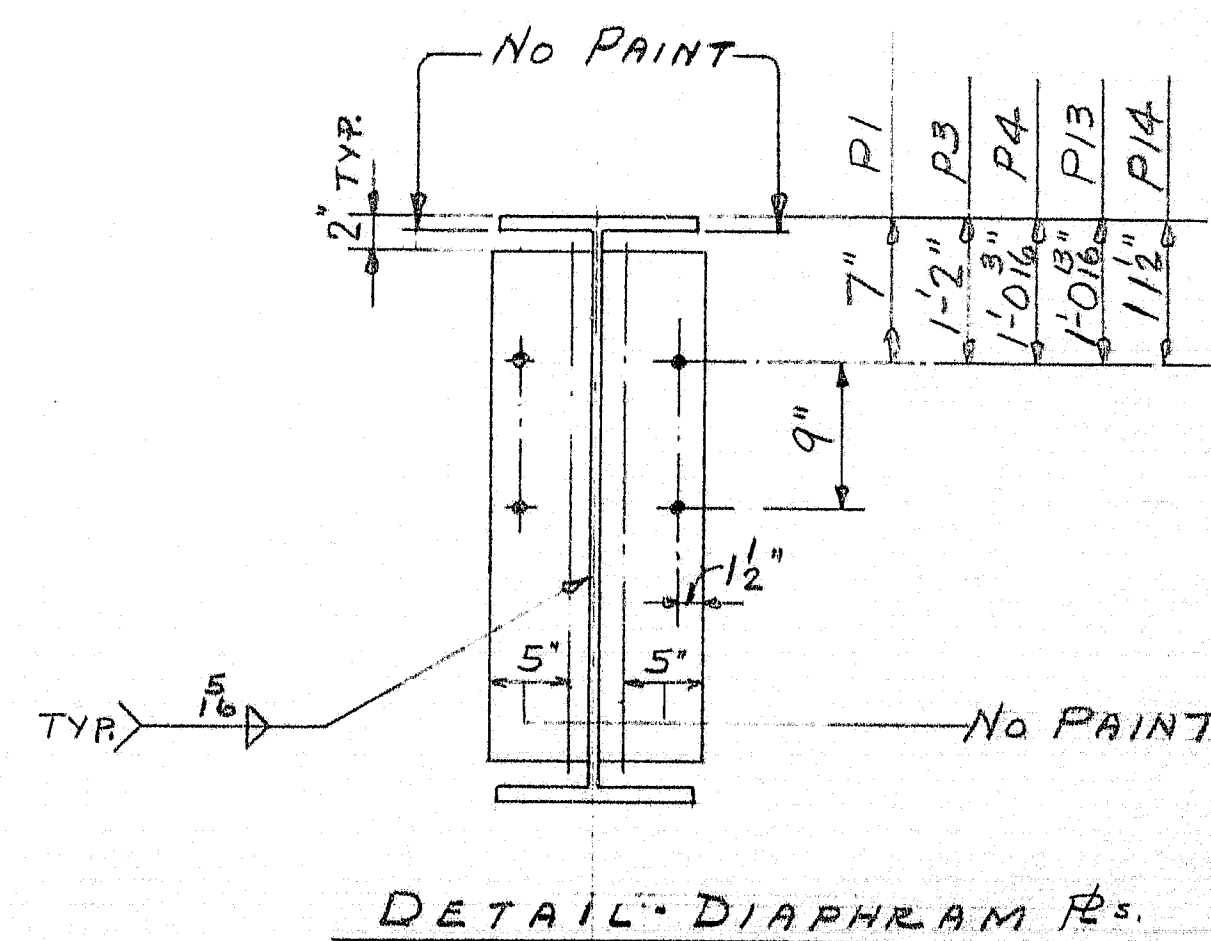
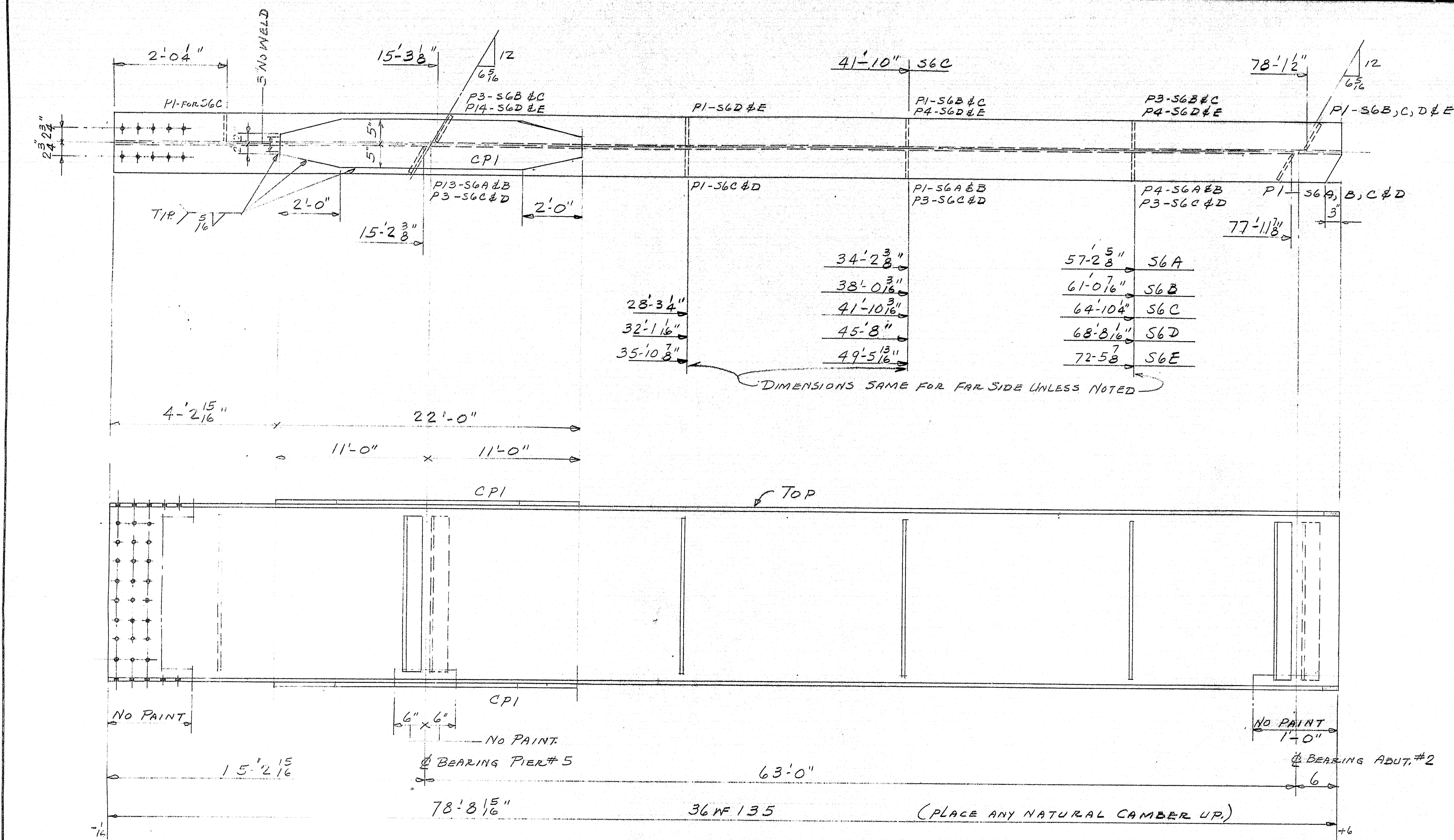
FIELD CONNECTIONS: 3" φ H.S. BOLTS

HOLES: 1 1/2" SEE NOTE 1 - 2

PAINT: STATE SPECS.

APP. AS NOTED 9-28-62

S4 - STRINGERS	
PRINT ISSUE	<u>Ramcraft & Martin Rolling Mills Company</u> <u>Brewer, Maine</u>
3 ERECT 10-9-62	<u>ROUTE 69 BRIDGE</u> <u>NEWBURGH MAINE</u>
5 SHOP 10-9-62	
2 F.A. 9-29-62	CUSTOMER <u>CALLAHAN BROS.</u> DESIGNER <u>STATE HIGHWAY DEPT.</u>
DRAWN 9-22-62 E.M.	
REVISION 10-9-62 E.M.	
REVISION	
REVISION	ORDER <u>VERBAL</u> DWG. <u>B62-168-S7</u>



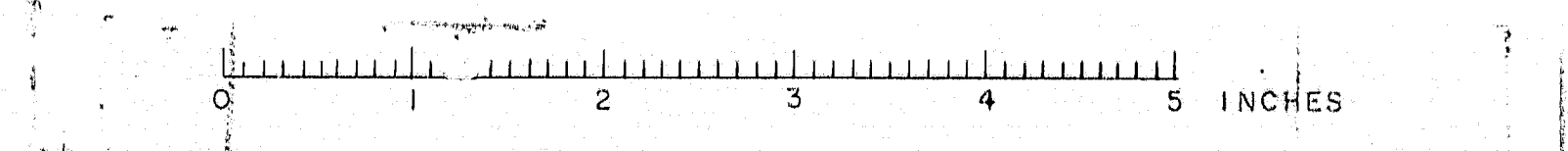
SHIP		BILL OF MATERIAL				DWG. B62-168-5	
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS	
S6A	1		36" x 135"	78' 8 15/16"	15		A36
S6B	1		Do	78' 8 15/16"	15		
S6C	1		Do	78' 8 15/16"	15		
S6D	1		Do	78' 8 15/16"	15		
S6E	1		Do	78' 8 15/16"	15		
	10	CPI	R 10 x 1	22' 0"	410		
	17	P1	36" x 6" x 3/8"	2' 7 1/2"	24		A7
	10	P3	Do	2' 7 1/2"	14		
	6	P4	Do	2' 7 1/2"	12		
	2	P13	Do	2' 7 1/2"	4		
	2	P14	Do	2' 7 1/2"	4		
	1		WELD	636	0		3.66 #/FT.

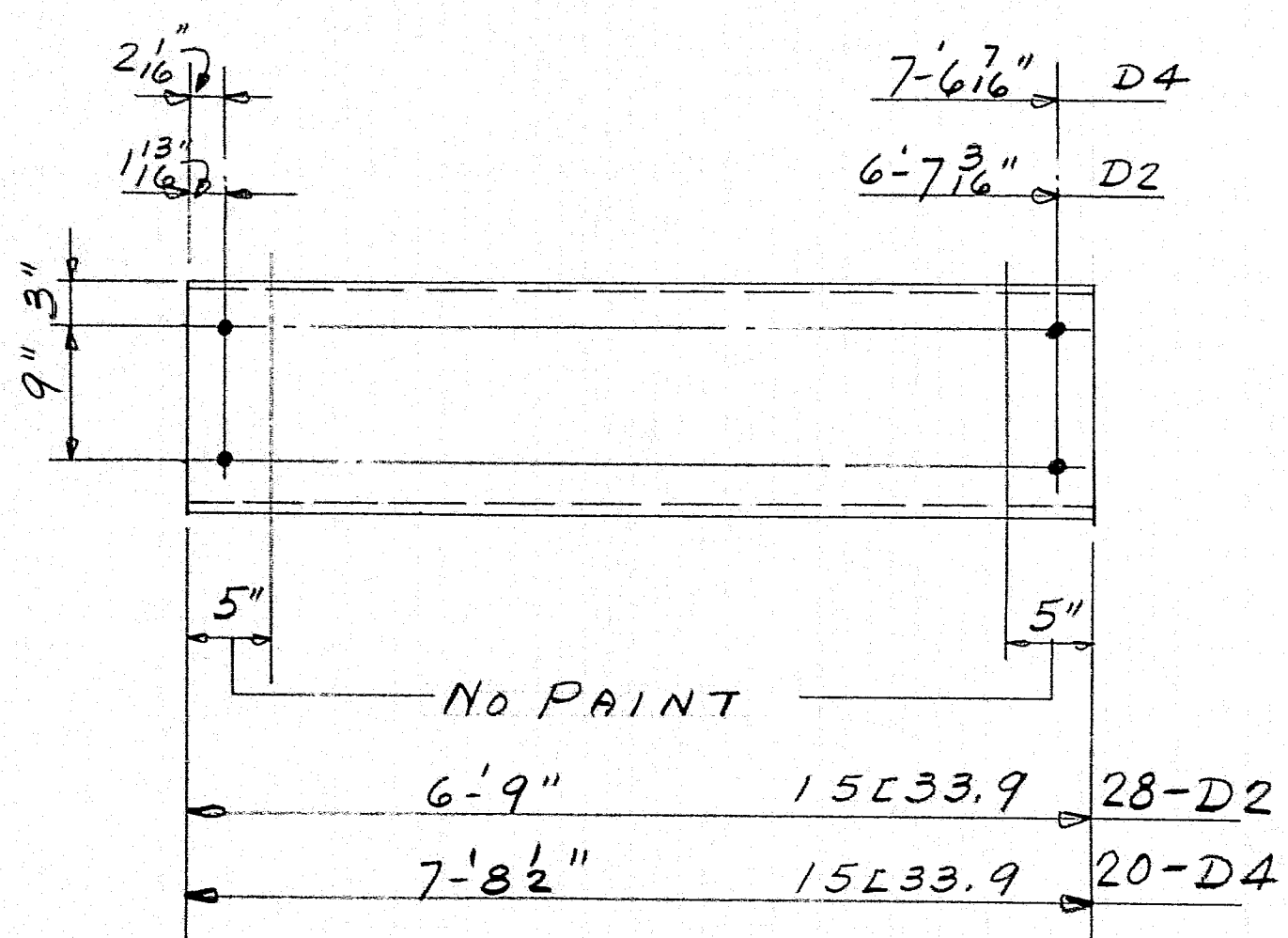
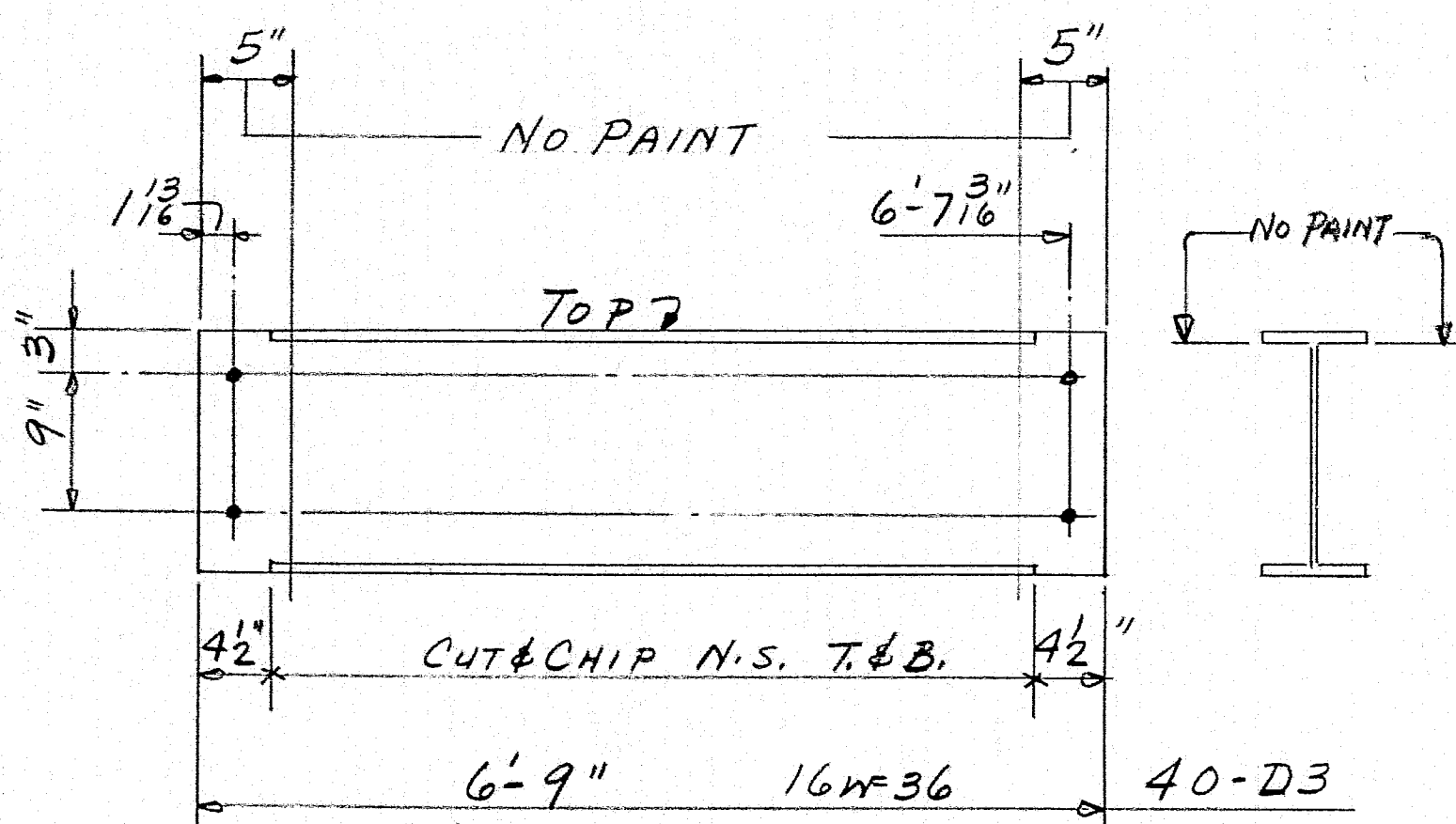
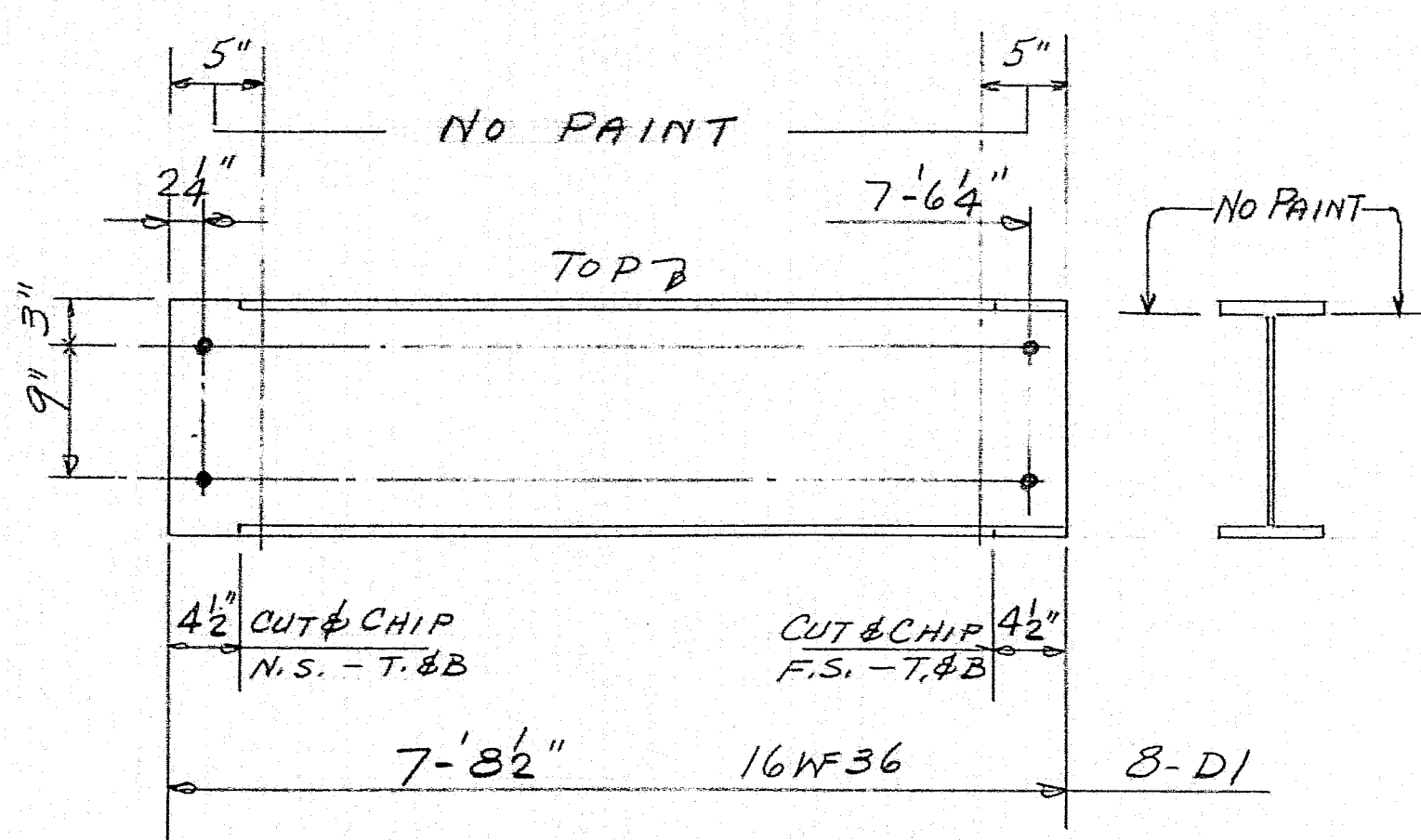
SHOP CONNECTIONS: WELD
FIELD CONNECTIONS: 3/8" H.S. BOLTS
HOLES: 1/8" - SEE NOTE ON DRAWING S4
PAINT: STATE SPECS.

APP AS NOTED 9-28-62

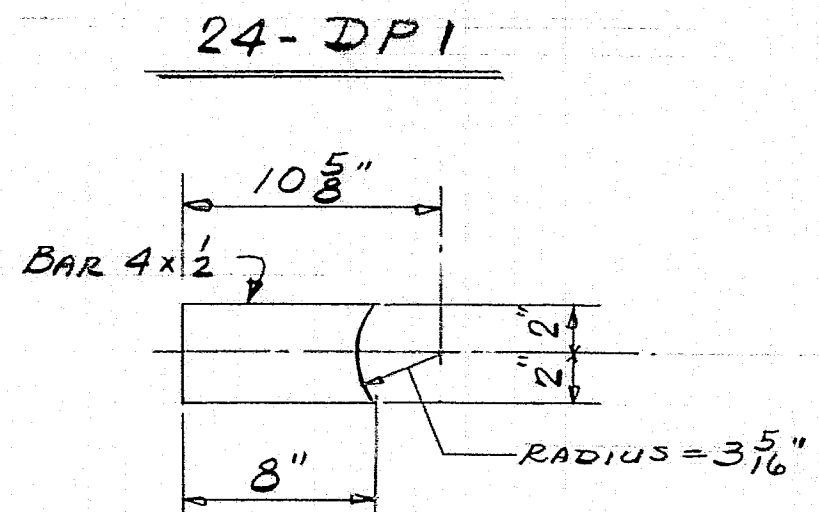
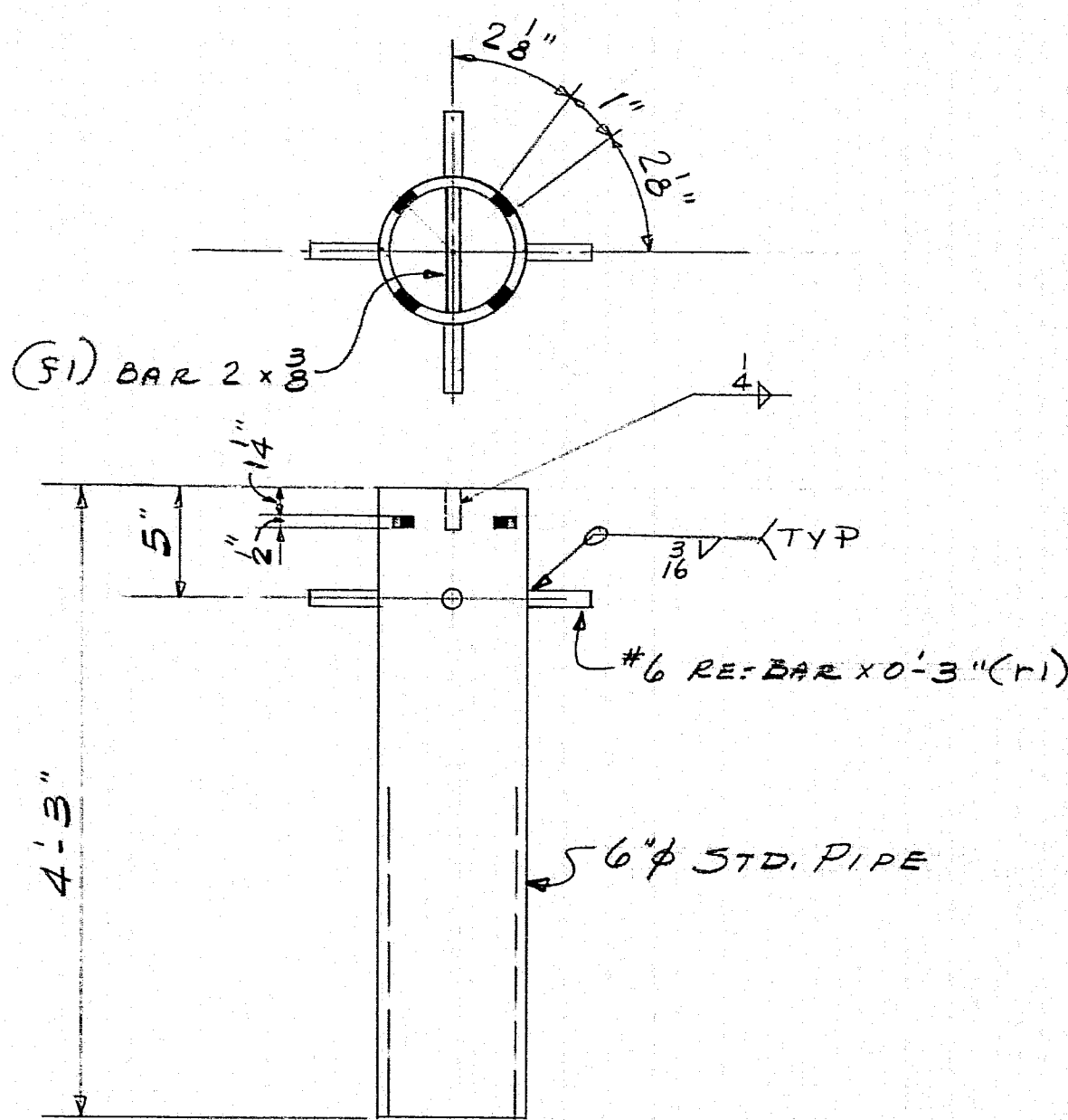
S6 - STRINGERS	
PRINT ISSUE	Bancroft & Martin Rolling Mills Company Brewer, Maine
	ROUTE 69 BRIDGE NEWBURGH MAINE
3 ERECT 10-9-62	CUSTOMER CALLAHAN BROS.
5 SHOP 10-9-62	DESIGNER STATE HIGHWAY DEPT.
DRAWN 9-22-62 E.M.	
REVISION 10-9-62 E.M.	
REVISION	ORDER VERBAL
REVISION	DWG. B62-168-5

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SHIP		BILL OF MATERIAL					DWG. B62-168-S10	
MARK	NO.	MARK	SHAPE	LENGTH	WT.	DEDUCT	REMARKS	
D1	8		16WF36	7' 8 1/2"		62#	A7	
D3	40		DO	6' 9"		310.5#	}	
D2	28		15L33.9	6' 9"		9.5#		
D4	20		DO	7' 8 1/2"		7#		
							↓	
DP1	24		6" STD PIPE	4' 3"		4#		
FB1	24		BAR 4x2	0' 8"		42#		
	24	FI	BAR 2x3	0' 6"				
	96	FI	3/4" PRE-BAR	0' 3"				

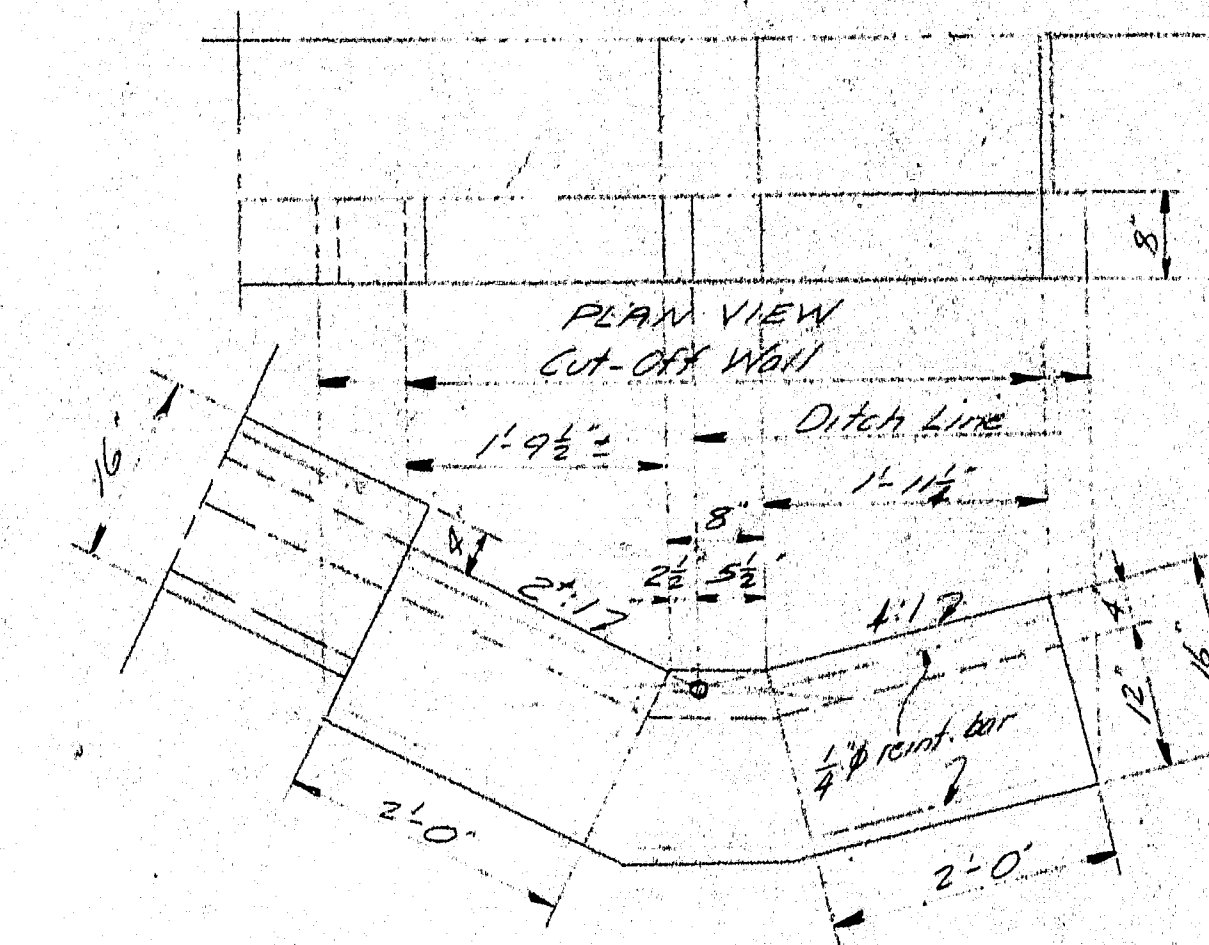
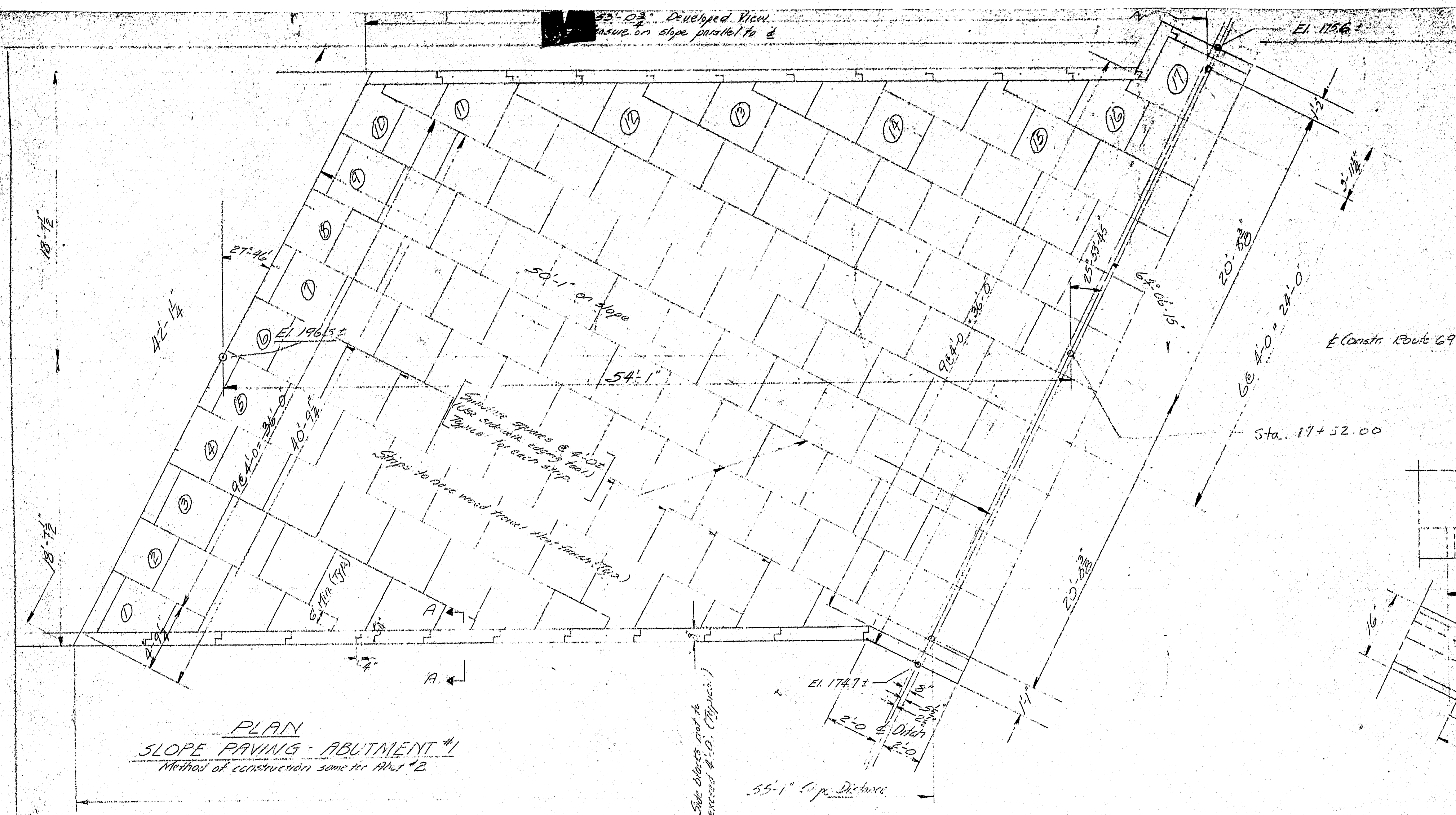


SHOP CONNECTIONS: WELD
FIELD CONNECTIONS: WELD
HOLES: 1/8" φ
PAINT: STATE SPECS.

APP. AS NOTED 9-28-62

DIAPHRAGMS & DRAINS			
PRINT ISSUE		Bancroft & Martin Rolling Mills Company	
		Brewer, Maine	
3 BRECT 10-9-62		ROUTE 69 BRIDGE	
5 SHOP 10-9-62		NEWBURGH MAINE	
2 F.A. 9-24-62		CUSTOMER CALLAHAN BROS.	
DRAWN 9-22-62 E.M.		DESIGNER STATE HIGHWAY DEPT.	
REVISION 10-9-62 E.M.		ORDER VERBAL	
REVISION		DWG. B62-168-S10	

85-176 M

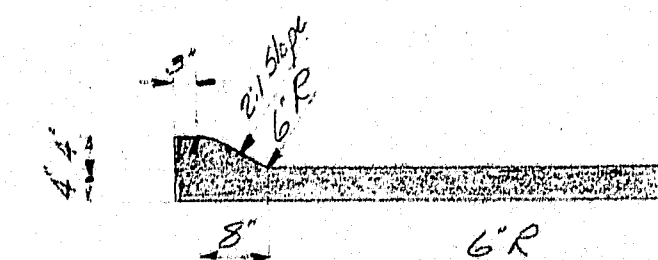
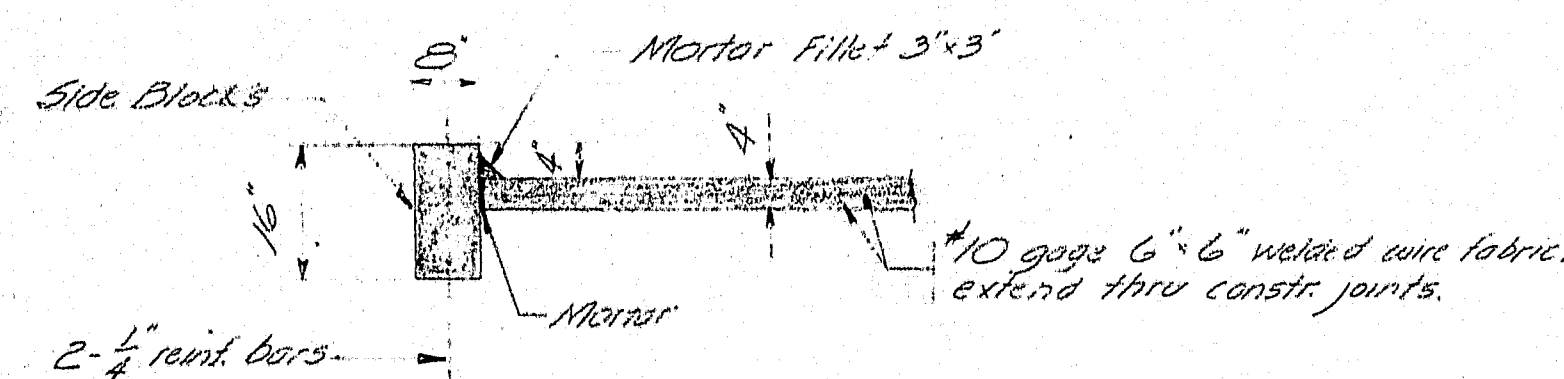


CAST IN PLACE DITCH CUT-OFF WALL
TYR BOTH ABUTS

Note: Inside form may be left in place

NOTE

Wetted white fabric to extend thru joints.
Break bond at joints with a coat of asphalt paint.



Cast in place concrete strips to be placed in alternate bays 1-3-5-7-9-11-13-15-17. When forms are removed place 2-4-6-8-10-12-14-16. Side blocks are to be precast and placed after forms for concrete strips are removed.

PLUM - MINNELL
RHECK 8626

STATE HIGHWAY COMMISSION
BEDECK DIVISION
ROUTE 67 BEDECK
OVER
INTERSTATE HIGHWAY 95
IN THE TOWN OF
NEWBURGH
PENOBSCOT COUNTY
CASTLENAPE BLOPP, PAUL
JULY 1972 - BUREAU OF HIGHWAYS

